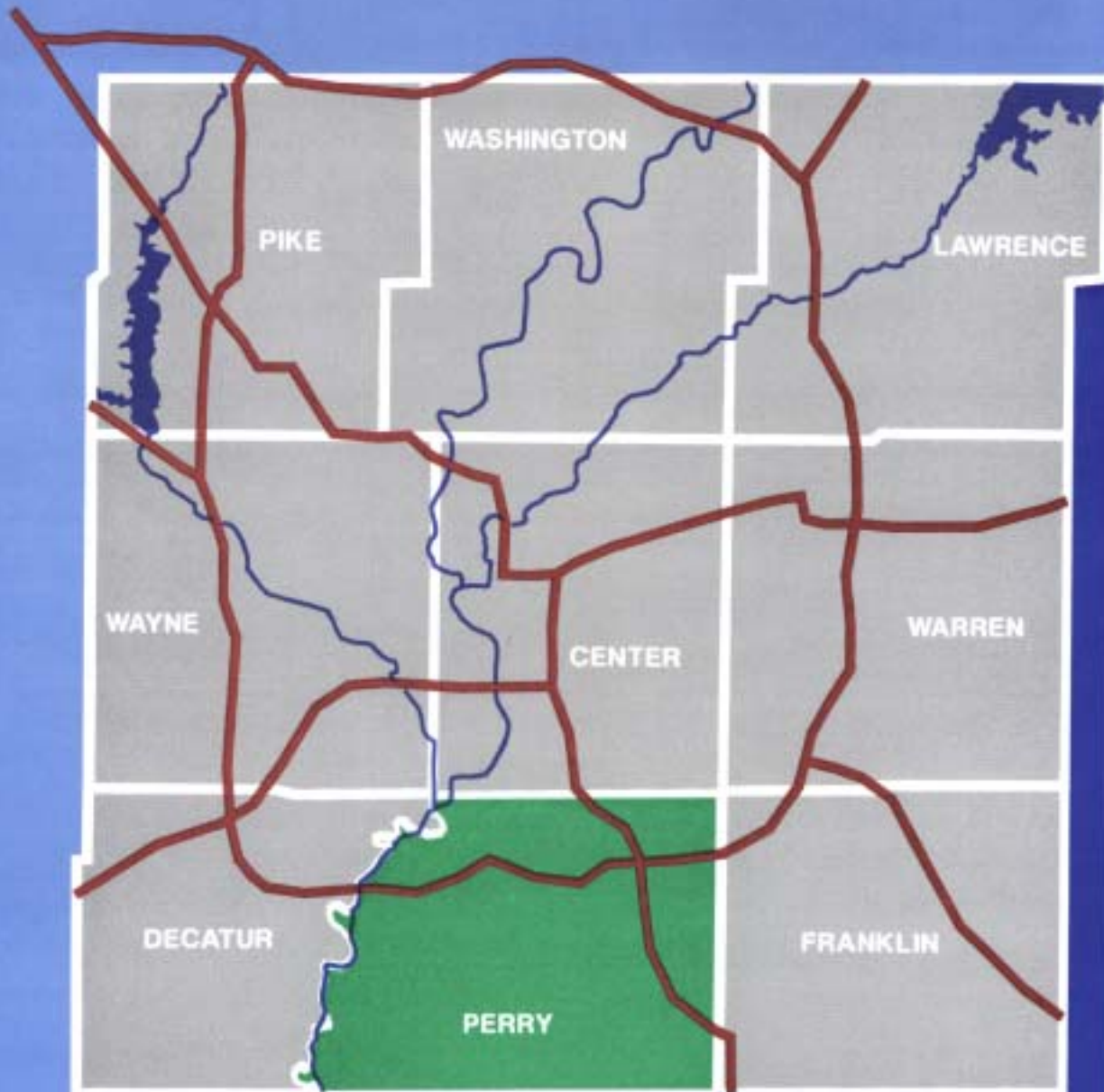


PERRY TOWNSHIP



COMPREHENSIVE LAND USE PLAN

92-CPS-R-5

ADOPTED SEPTEMBER 16, 1992

DEPARTMENT OF METROPOLITAN DEVELOPMENT
DIVISION OF PLANNING
CITY OF INDIANAPOLIS-MARION COUNTY, INDIANA



STEPHEN GOLDBARTH

Perry Township

Comprehensive Land Use Plan



Prepared by:

Department of Metropolitan Development

Division of Planning

Indianapolis-Marion County, Indiana

Adopted: September 16, 1992

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Perry Township Comprehensive Plan

Introduction

Perry Township is located between Decatur and Franklin Townships in the southern portion of Marion County. The township has many features which make it unique in comparison to other Marion County townships. These features include the White River; the U.S. 31/Madison Avenue corridor commercial area; a large underground aquifer; numerous operating and non-operating aggregate mining businesses; and large areas of undeveloped land.

Township characteristics are identified in more detail in a background information document entitled *Perry Township Comprehensive Planning Study Data Inventory*. These characteristics include: population make-up; land use and zoning changes; and roadway and infrastructure needs. The Department of Metropolitan Development's Division of Planning compiled background information in preparation of the Comprehensive Plan revision. The *Data Inventory* was used during the planning process to examine development trends, identify areas with infrastructure services, environmental constraints, and thoroughfare needs. A Township Planning Committee was formed to generate ideas and make recommendations for land use changes. The planning committee meetings were conducted in 'town meeting' fashion and any individual who chose could attend. The planning process for Perry Township included six committee meetings which resulted in the plan recommendations contained in the Land Use Plan Map and this plan document.

The 1992 Perry Township Comprehensive Plan revision, a part of the Comprehensive Plan for Marion County, is comprised of this narrative, its accompanying Perry Township Comprehensive Land Use Plan Map, and the Official Thoroughfare Plan for Marion County (1991). This Perry Township narrative is intended to be used with the other parts of the 1991 Comprehensive Plan for Marion County, including the narrative portion of the county-wide plan.

Township Population and Land Use Characteristics

The following Perry Township characteristics were identified in the *Perry Township Comprehensive Planning Study Data Inventory* and planning committee meetings. The Comprehensive Plan responds in some way to each of these characteristics.

- *Perry Township is just over one-half developed.*
 - In 1990, Perry Township ranked sixth highest in population (85,060) of all townships in Marion County. The township's overall density, in terms of

persons per acre, was also the sixth highest in Marion County in 1990 at 2.93. This compares to the lowest, Franklin Township (0.81 persons per acre) and the highest, Center Township (5.0 persons per acre).

- Almost 50% of the township is undeveloped.

Comprehensive Plan Response - The Comprehensive Plan recommends uses for all township land, although parts of the township could remain undeveloped throughout the next century. This plan recommends about the same proportion of each type of land use as the 1984 Comprehensive Plan (see Table 1).

■ *The township's population is growing slowly.*

- According to the 1980 Census, Perry Township averaged 2.7 persons per household. By 1990 the figure was 2.5. Generally, more people per household means more children per-household. However, the average number of persons per household in Perry Township declined between 1980 and 1990 suggesting smaller family groups and/or more single person households.
- The number of persons under 18 years old made up 25% of the 1990 Census population for Perry Township, compared to 31% in 1980. This supports the notion that Perry Township has smaller family groups than in 1980.

Comprehensive Plan Response - The Comprehensive Plan's residential land use recommendations are divided into four categories on the basis of the number of residential dwellings per acre. The total acreage of each residential category, along with household size, and density of residential development will determine the potential total population of Perry Township when it is fully developed. Based upon the land use designations and future conditions, school enrollments and township needs can be projected.

■ *Most of the development in Perry Township has been concentrated along Madison Avenue and U.S. 31. However, the Southport Road and Interstate 65 area has recently exhibited concentrated growth.*

- Between 1960 and 1985, over 70% of the township's single-family home construction occurred in the southeast section of the township. This development was a combination of new subdivision development, the expansion of existing subdivisions, and one-acre single family residential tracts along existing roads.

Table 1

Perry Township Comparisons of Existing Land Use and Comprehensive Plan Recommendations

Land Use Category	1985 Existing Land Use		1984 Comprehensive Plan Land Use Recommendations		1991 Comprehensive Plan Land Use Recommendations	
	Acres	% of Twp.	Acres	% of Twp.	Acres	% of Twp.
Residential						
Single & Two Family	9,415	32.4	18,245	62.9	16,488	56.8
Medium Density	753	2.5	2,666	9.2	1,676	5.7
Total	10,168	35.0	20,911	72.1	18,164	62.5
Commercial						
Office	78	0.2	*	*	611	2.1
Retail	825	2.8	*	*	1,404	4.8
Total	903	3.1	1,447	5.0	2,015	6.9
Industrial						
Light	177	0.6	977	3.4	1,266	4.4
Heavy	981	3.3	1,960	6.8	1,264	4.3
Total	1,158	3.9	2,937	10.1	2,530	8.7
Public/Semi-Public**	2,747	9.4	3,722	12.8	6,308	21.7
Vacant or Agricultural	14,041	48.3	***	***	***	***
Total, All Categories	29,017	100.0	29,017	100.0	29,017	100.0

* The 1984 Comprehensive Plan did not differentiate between office and retail commercial uses.

** The Public/Semi-Public Land Use Category includes Urban Conservation areas.

*** The 1984 and 1991 Comprehensive Plans assume full development of the township, with no vacant or agricultural land.

- Sixty percent of the new rental units were located in the southeastern portion of the township. The western section of the township experienced no development of rental units.
- Future residential development will likely continue at the periphery of existing subdivisions, predominantly at the township's western and eastern extremes.

Comprehensive Plan Response - Development of land for all land use categories according to the Comprehensive Plan will ensure orderly township growth.

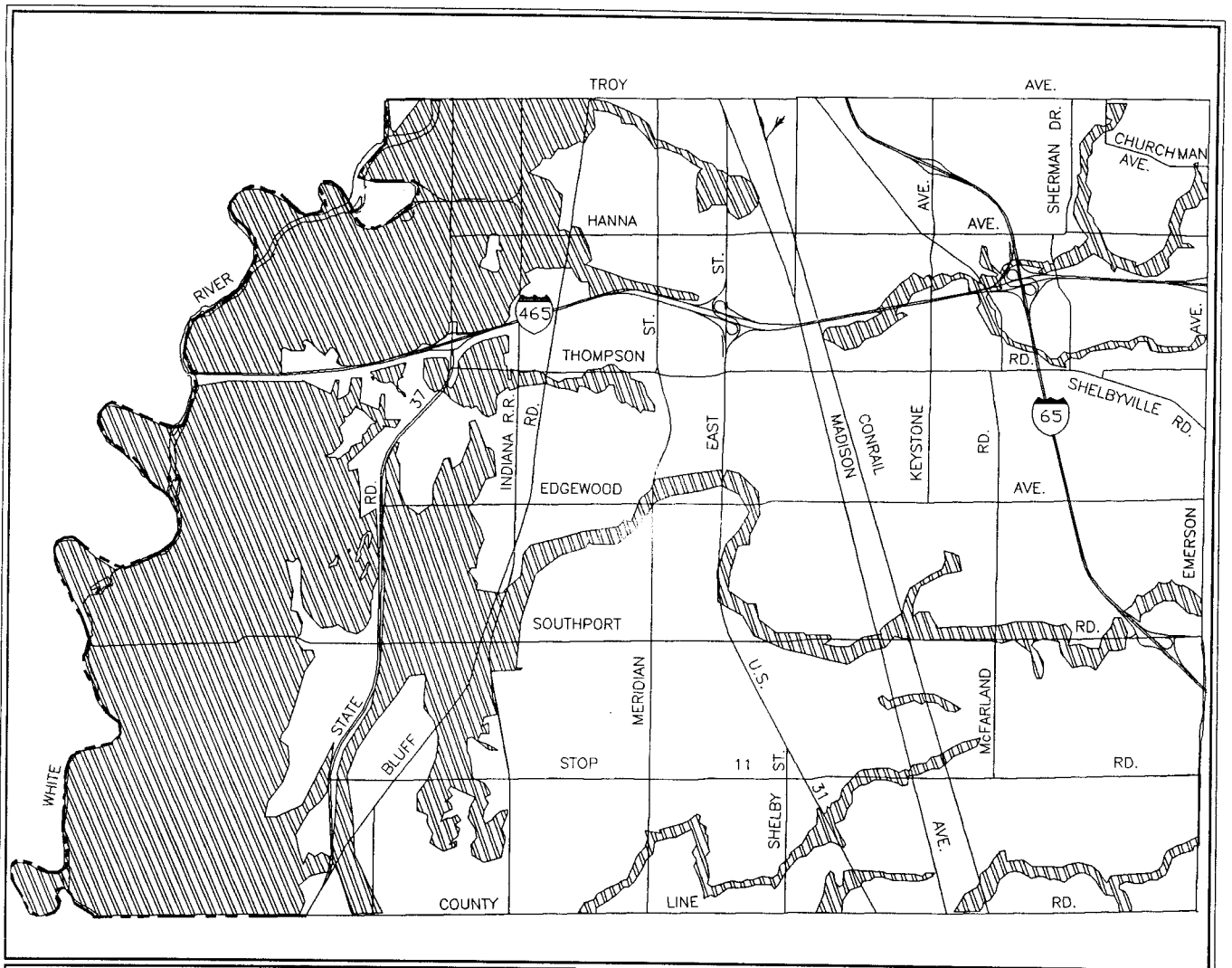
- *The township's roadway and sewer systems have under-utilized capacities but will need to be improved in order to serve additional development.*

- Overall, the Perry Township street system is currently operating with little traffic congestion. Streets that are primary commuter routes for downtown destinations or that provide access to the interstate system or intense commercial uses are operating with higher levels of traffic congestion.
- The southwest portion of Perry Township has the probability to become a significant source of water for the Indianapolis Water Company which serves the Indianapolis metropolitan area. Septic effluent that is not completely absorbed by the soil could contaminate a potential source of water supply for the community. A logical preventive measure is the extension of the Indianapolis Public Sewer System into the aquifer recharge area.

Comprehensive Plan Response - The Comprehensive Plan recommends land uses appropriate to the future capacities of the infrastructure -- i.e., the roadway, sewer, and water systems. The plan also recommends that all new developments provide or make commitments for the provision of infrastructure necessary to serve new developments.

- *Perry Township contains precious resources including an underground aquifer, the White River, wetlands, wooded areas, and aggregate resources.*

- The White River is particularly prone to flooding. In addition, its floodplain contains significant wetland and wooded areas (see Maps 1, 2, and 3).
- Little development has occurred to date along and in the White River floodplain, which increases the feasibility of establishing linear parks along its banks.



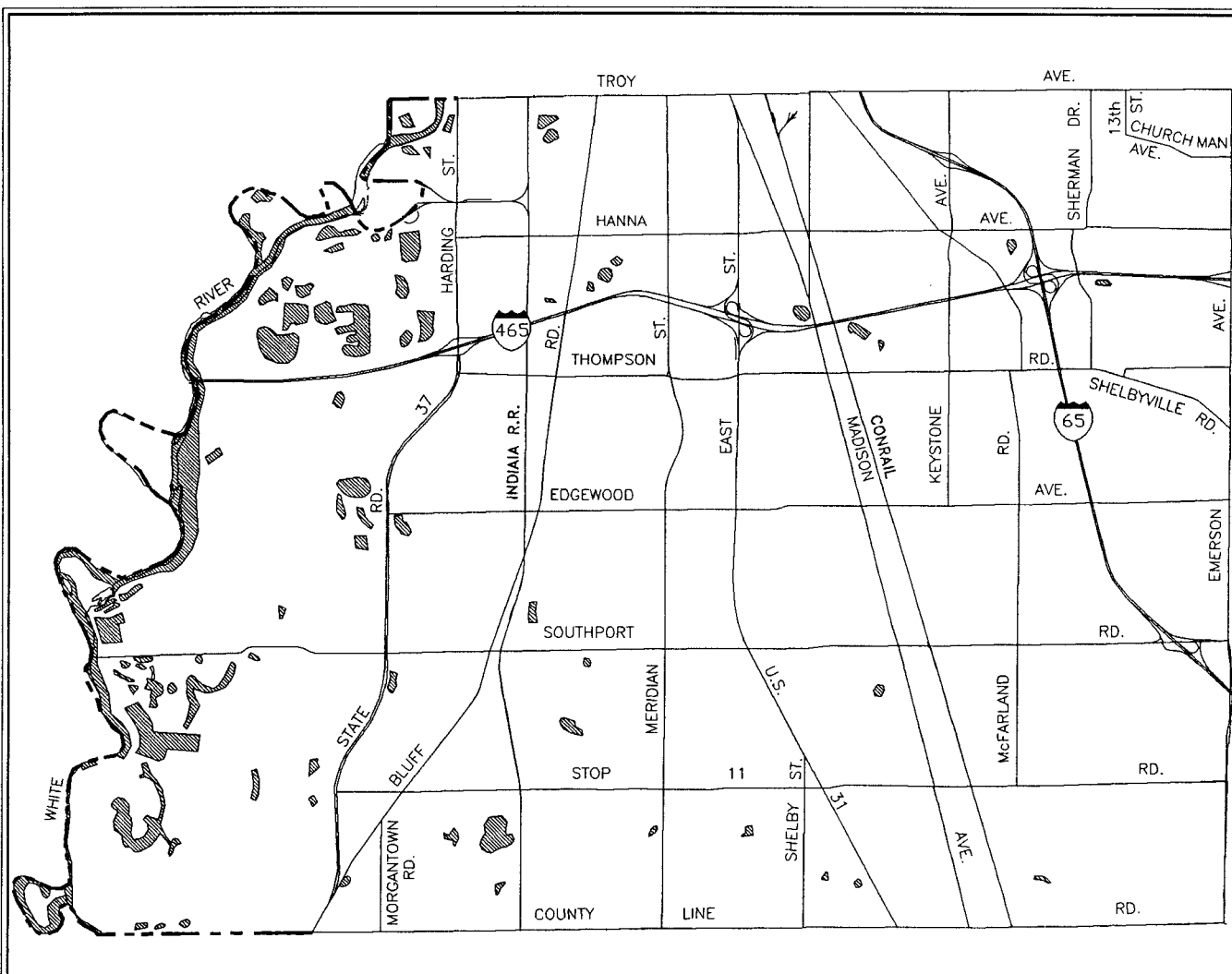
MAP 1
PERRY TOWNSHIP
GENERALIZED FLOODPLAINS


100-YEAR FLOOD BOUNDARY

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MAP 2
PERRY TOWNSHIP
GENERALIZED WETLANDS

■ WETLAND AREAS

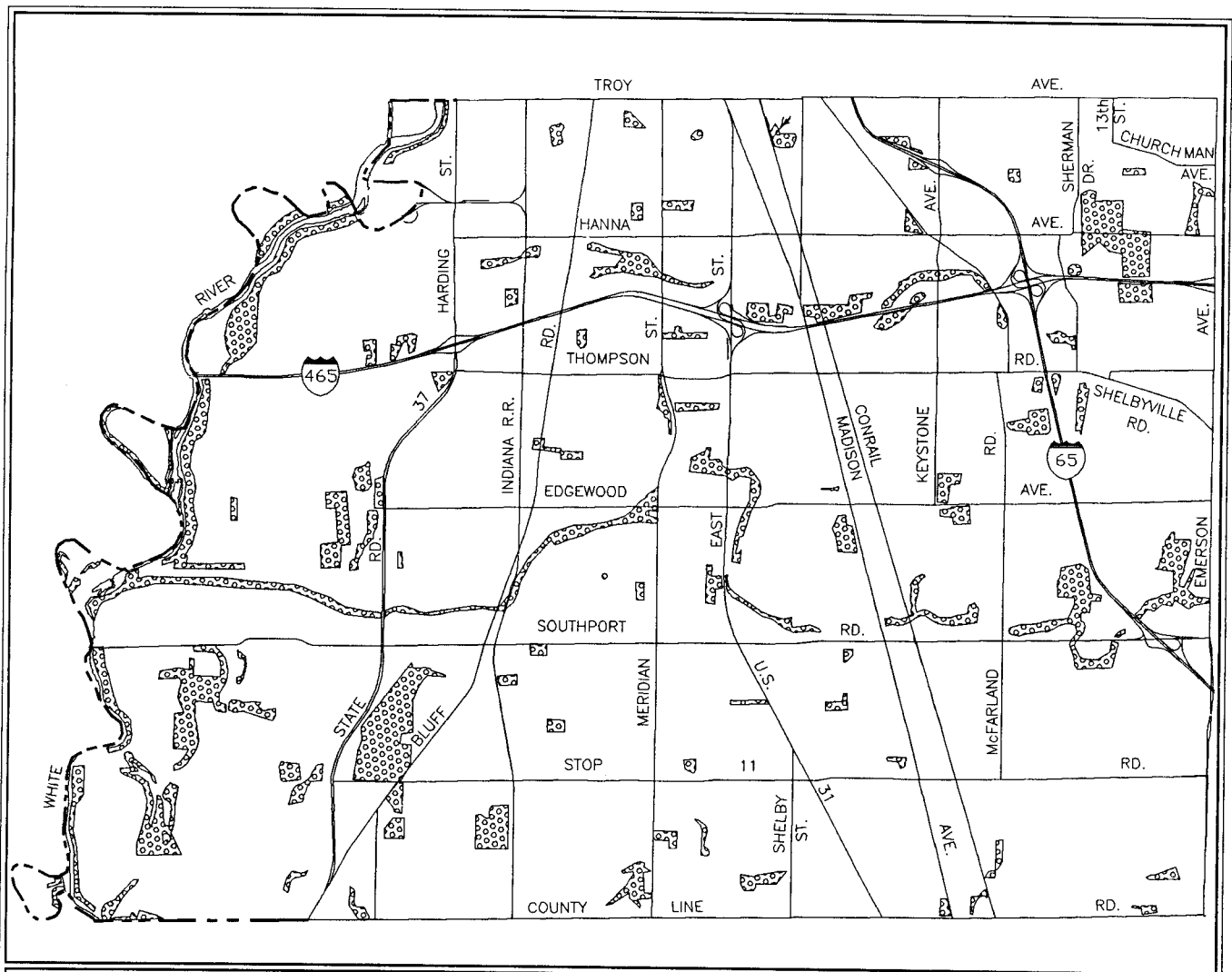
SOURCE: U.S. FISH AND WILDLIFE SERVICE NATIONAL WETLANDS INVENTORY, 1990

THIS MAP IS NOT EXHAUSTIVE. IT INCLUDES MOST NON-STREAM WETLANDS
OF TEN ACRES OR MORE. FOR MORE COMPLETE INFORMATION REFER TO THE
NATIONAL WETLANDS INVENTORY OR THE U.S.D.A. SOIL CONSERVATION SERVICE.

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MAP 3

PERRY TOWNSHIP

GENERALIZED WOODLANDS

 WOODED AREA

SOURCE: DIVISION OF PLANNING AERIAL PHOTOGRAPHS, APRIL, 1991

NOTE: THIS MAP IS NOT EXHAUSTIVE. IT INCLUDES MOST WOODLANDS OF TEN ACRES OR MORE.

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- The Indianapolis Water Company has established well fields in Perry Township which will provide future drinking water for the metropolitan Indianapolis community.
- Aggregate mining (open pit excavation of sand and gravel) and removal of top soil is pervasive in the northwestern portion of the township.

Comprehensive Plan Response - The Comprehensive Plan recommends establishing linear parks along the White River. The plan recommends other park uses, along with Urban Conservation and Low and Very Low Density Residential development, in areas adjacent to the river. The plan strongly discourages development in wetland, floodway, and wooded areas by recommending nature conservation, park uses, or low density cluster residential development. The plan also discourages septic systems within the boundaries of the aquifer.

Also, the plan strongly discourages commercial or industrial land use within the boundaries of the aquifer protection area (see Critical Area 2, page 24). Uses such as gasoline sales, service stations, and dry cleaners - all of which may store and/or use harmful chemicals, solvents, and other potentially hazardous materials which would threaten the quality of the groundwater would not be permitted directly above or near the aquifer area.

■ *Between 1972 and 1985, commercial land use development increased at a rate greater than that of any other land use category.*

- During that 13 year period, commercial growth was concentrated in the southeastern portion of the township (along Madison Avenue and U.S. 31) and industrial development increased slightly throughout the township. In 1985, commercial and industrial development totaled 552 acres.
- Commercial land uses in 1985 occupied 3.1% (326 acres) of the developed land in Perry Township. By comparison, commercial uses occupied 0.3% (71 acres) of developed land in Decatur Township and 0.6% (167 acres) of developed land in Franklin Township (in 1988).
- Nearly all industrial development is currently located near I-465 and Harding Street in the northwest portion of Perry Township. Future industrial development is expected to occur in the same area.

Comprehensive Plan Response - This Comprehensive Plan recommends that future commercial and industrial development occur near those areas currently developed for such uses.

Additionally, the Comprehensive Plan fully incorporates the recommendations and strategies for development contained in the *U.S. 31 and South Madison Avenue Corridor Plan* (adopted in May, 1988; see Map 4). That plan encourages controlled commercial development, elimination of physical and business deterioration, maintenance and expansion of housing stock surrounding the corridors, prevention of new and removal of existing industrial development in the area. The Corridor Plan also recommends improvements in transportation, public safety services, aesthetics of development in the corridors, and public facilities.

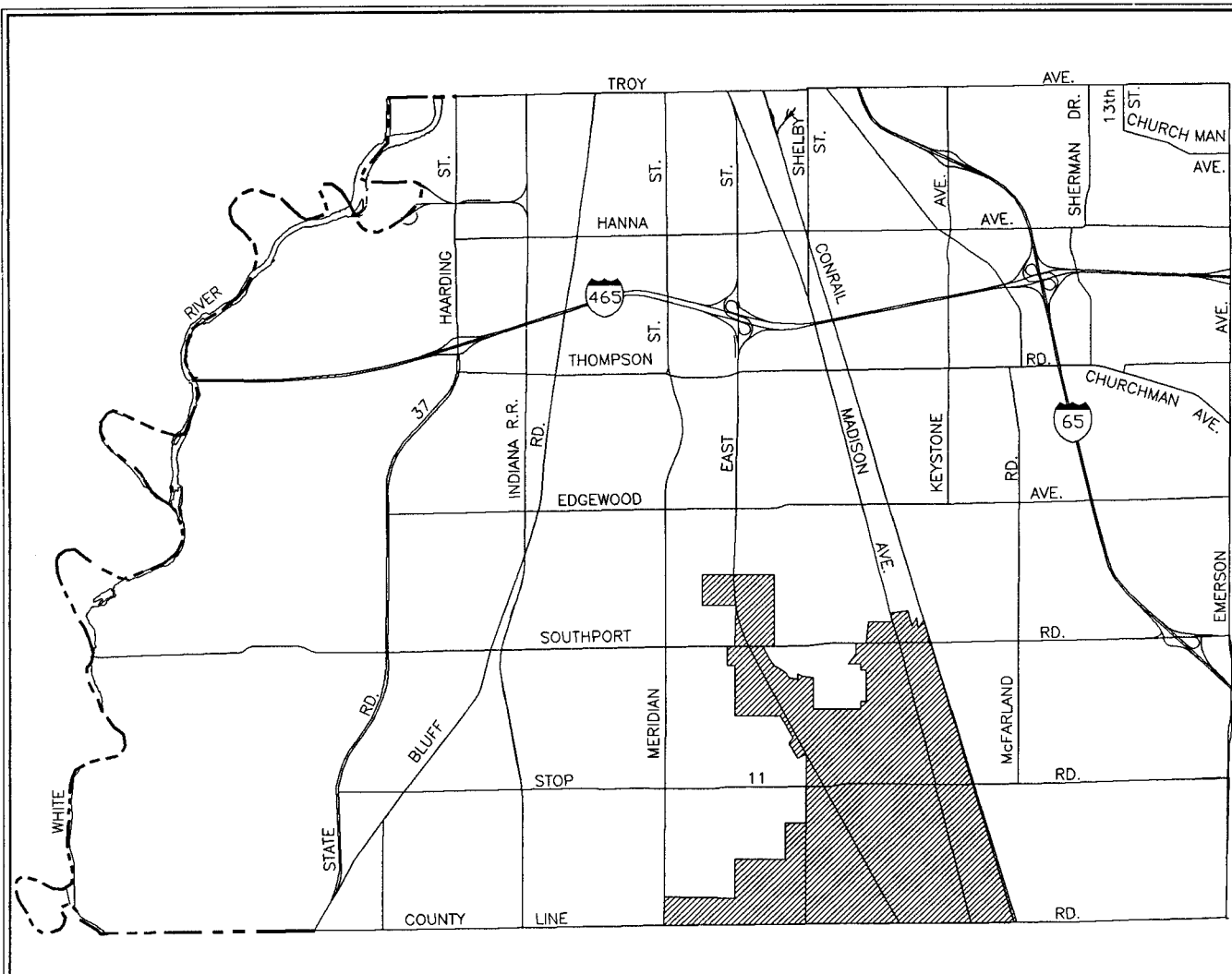
Township Planning and Development Goals

The Perry Township Planning Committee (see Appendix A) identified the following goal statements to be reflected in the Perry Township Comprehensive Plan revision:

- Protect the large underground water supply (the aquifer) and its associated well fields in the southwest portion of the township from potential contamination by regulating and eliminating industrial uses and regulating the commercial removal of top soil, subsoils, and aggregate deposits.
- Control commercial growth along South U.S. 31 and Madison Avenue.
- Reduce the traffic congestion along Meridian Street, U.S. 31, Madison Avenue, and Emerson Avenue.
- Expand office development along Interstate 465, Interstate 65, and Emerson Avenue.
- Maintain the residential character of the township.
- Construct more schools and establish additional park and open space areas for future population growth.

Thoroughfare Plan

Perry Township is included in the *Marion County Thoroughfare Plan*. The Thoroughfare Plan recommends roadway improvements designed to mitigate existing and future traffic congestion and delays, which are measures of the roadway system's efficiency (see Map 5). These improvements include right-of-way preservation needs. The Thoroughfare Plan assigns a priority to each recommended improvement indicating when the work should occur. Priorities range from "A" (highest priority) to "D" (lowest priority). Timing of implementation of the plan's recommendations by such agencies as Indianapolis Department



MAP 4

PERRY TOWNSHIP

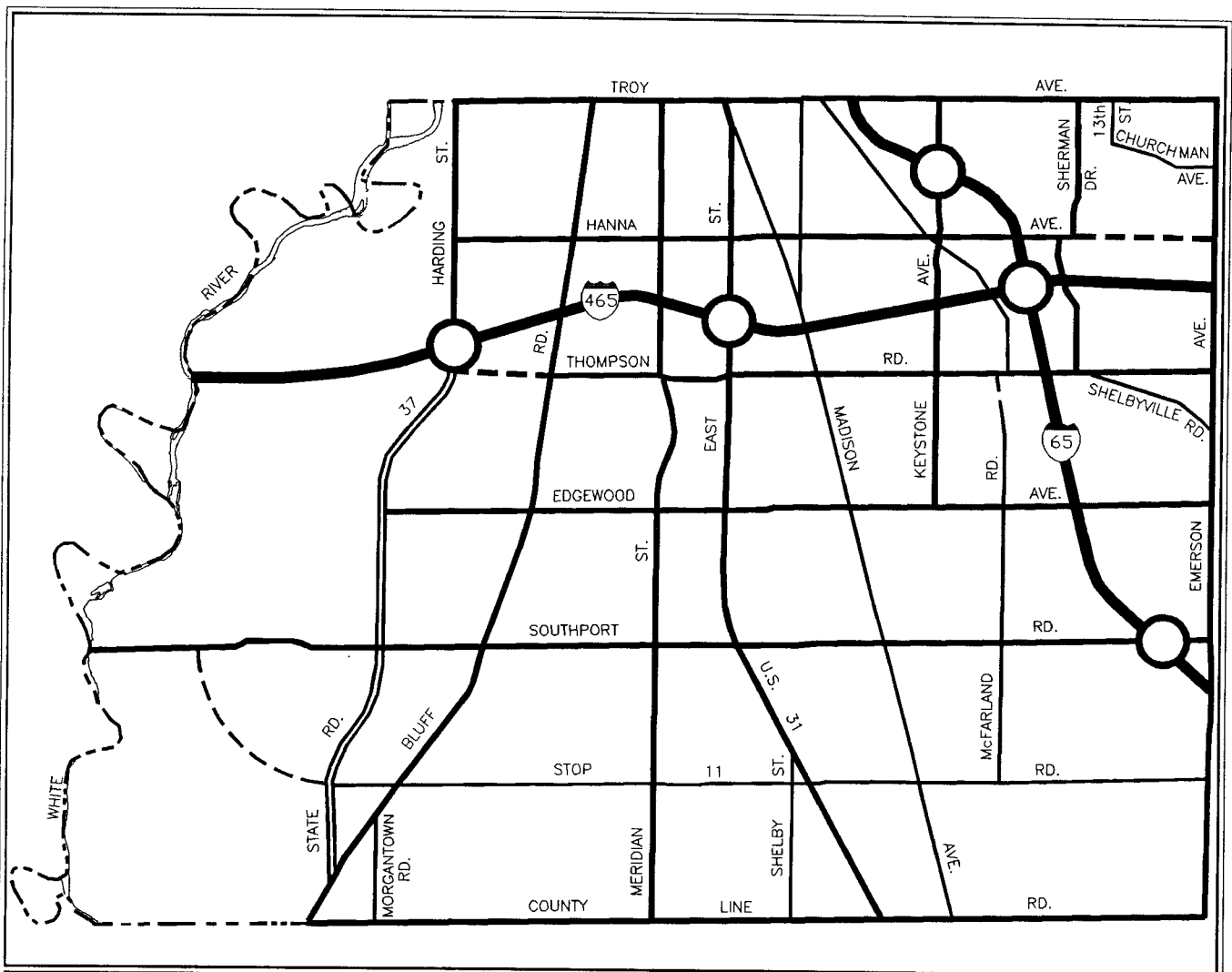
U.S. 31 AND SOUTH MADISON AVENUE CORRIDOR PLAN AREA

 SUBAREA LAND USE PLAN BOUNDARIES

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INDIANAPOLIS-MARION COUNTY, INDIANA



MAP 5
PERRY TOWNSHIP
PORTION OF THE OFFICIAL
THOROUGHFARE PLAN MAP, 1991

EXISTING	PROPOSED	
		INTERCHANGE
		FREEWAY
		EXPRESSWAY
		PRIMARY ARTERIAL
		SECONDARY ARTERIAL

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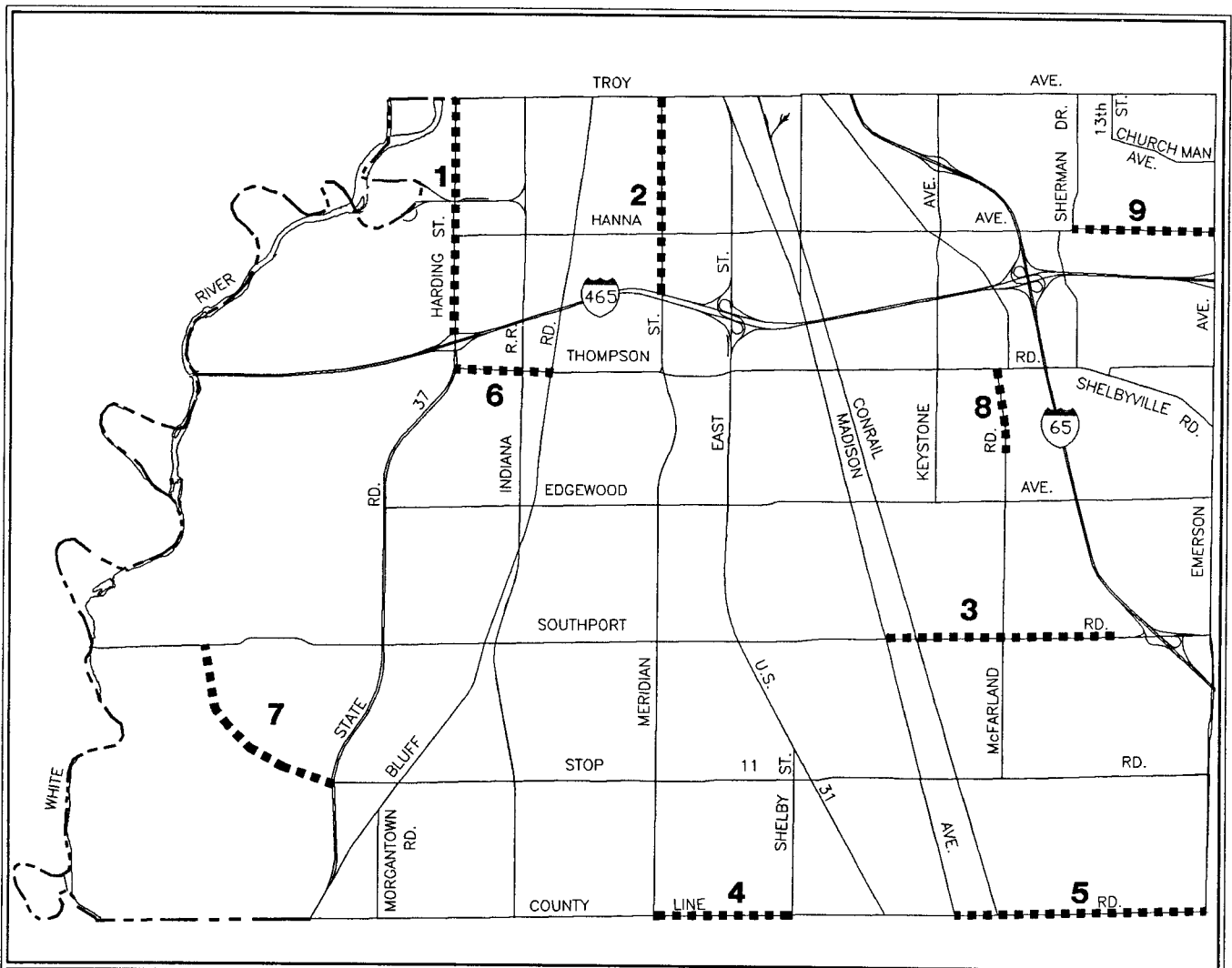
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of Transportation and Indiana Department of Transportation is a function of available funding and the priority schedule for improvements throughout Marion County.

None of the township's roadways are currently operating over capacity, although some are projected by the Division of Planning Transportation Planning Section to do so in the future. To increase the efficiency of the roadways and to prevent their reaching or exceeding capacity in the future, the Thoroughfare Plan recommends the following priority improvements (see Map 6):

1. Four-lane primary arterial of Harding Street from Troy Avenue to Interstate 465, Priority A.
2. Four-lane primary arterial of Meridian Street from Troy Avenue to Southern Plaza Drive, Priority A.
3. Four-lane primary arterial connector of Southport Road from Madison Avenue to Gray Road, Priority A.
4. Four-lane primary arterial connector of South County Line Road from Meridian Street to Shelby Street, Priority B.
5. Four-lane primary arterial connector of South County Line Road from Madison Avenue to Emerson Avenue, Priority C.
6. Four-lane primary arterial connector of Thompson Road from Bluff Road to State Road 37, Priority D. Initially this would be constructed as a two-lane roadway until traffic demand warrants four lanes.
7. Four-lane secondary arterial connector of Stop 11 Road from Southport Road to Belmont Avenue, Priority D. Initially this would be constructed as a two-lane roadway until traffic demand warrants four lanes.
8. Four-lane secondary arterial connector of McFarland Road from Thompson Road to Midland Road, Priority D. Initially this would be constructed as a two-lane roadway until traffic demand warrants four lanes.
9. Four-lane primary arterial connector of Hanna Avenue from Sherman Drive to Emerson Avenue, Priority D. Initially this would be constructed as a two-lane roadway until traffic demand warrants four lanes.

Perry Township land use recommendations are based in part on the Thoroughfare Plan's priority improvements and also on the levels-of-service of roadways.



MAP 6

PERRY TOWNSHIP

THOROUGHFARE PLAN PRIORITY IMPROVEMENTS

..... PRIOROTY IMPROVEMENTS

PRIORITY A

1. HARDING STREET WIDENING
2. MERIDIAN STREET WIDENING
3. SOUTHPORT ROAD WIDENING

PRIORITY B

4. SOUTH COUNTY LINE ROAD WIDENING

PRIORITY C

5. SOUTH COUNTY LINE ROAD WIDENING

PRIORITY D

6. THOMPSON ROAD CONNECTOR
7. STOP 11 ROAD CONNECTOR
8. McFARLAND ROAD EXTENSION
9. HANNA AVENUE EXTENSION

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"Level-of-service" is a measure of traffic congestion that rates roadways from A (least congested) through F (most congested). This measure identifies deficiencies in the roadway network. For example, a roadway segment with a level-of-service E or F is carrying more traffic than it is designed to carry.

The type and density of land use in an area determines the amount of traffic generated by the area. In turn, the amount of traffic generated affects roadway levels-of-service. Thus, the type and density of land use affects roadway levels-of-service. For example, traffic generated per acre of development is usually higher for commercial centers than for residential uses, and higher for multi-family residential uses than for single-family residential uses. In general, the more traffic generated by a land use, the greater the effect on roadway levels-of-service. The land uses recommended in this plan reflect existing and projected roadway levels-of-service. Therefore, deviations from this plan's land use recommendations will inevitably alter the roadway levels-of-service.

Stages of Development

The Comprehensive Plan for Marion County differentiates areas of the county by their history, rate of development, and pressure for growth -- their stages of urban development. The entire county is comprised of stages numbered from Stage 1 which is the most intensely developed, to Stage 7 which contains the least amount of development.

Perry Township contains areas in Development Stages 2, 3, 4, 5, 6, and 7. The location and development policies for each stage are as follows (see Map 7, and refer to the *Marion County Comprehensive Plan* for additional information on Stages of Development):

- *Stage 1 - Not present in Perry Township*
- *Stage 2 (Center City Revitalization Area)*

Only a small portion of Perry Township is identified as Stage 2 -- the southern half of the Belmont Waste Facility, west of the White River. This area is identified as Stage 2 due to the area north of the waste facility which developed in the early 1900's. The Belmont Waste Facility will likely continue to function at this location well into the 21st Century.

- *Stage 3 (Established Center City Area)*

The northeast corner of Perry Township (part of Beech Grove) is identified as Stage 3. This area contains some of the oldest subdivisions in the township. Community

services are in place but some facilities may need reconstruction or replacement due to age. Roadway services are generally adequate except for traffic congestion at intersections during peak hours of traffic. The area has a stable neighborhood and is experiencing restoration and reinvestment.

- *Stage 4 (Suburban Revitalization Area)*

Most of the north half of Perry Township is characteristic of Stage 4. The boundaries of this area are: Troy Avenue, Keystone Avenue, Interstate 465, and Harding Street. Many subdivisions, industrial, and commercial developments have been established in this area. Maintain these uses as separate units of activity.

Also, a small portion of western Perry Township, bounded by the White River and Decatur Township, is included in Stage 4. That area has been used for gravel/sand extraction. When that use ceases redevelop the area for a more appropriate use.

Stage 4 areas are beginning to show signs of deterioration in their roadway infrastructure. Commercial areas can be improved by development of infill sites, strengthening adjacent residential neighborhoods and enforcing commercial zoning ordinance standards.

- *Stage 5 (Established Suburban Area)*

The south-central and middle north-eastern portion of Perry Township demonstrates Stage 5 characteristics. It extends as far north as Troy Avenue but is mostly the central portion of the township, south of Interstate 465. The eastern boundaries of this Stage 5 area are made up of Stage 3, Interstate 65, and Sherman Drive. The southern most boundary is South County Line Road. The western edge is Bluff Road and Stage 4.

Stage 5 in Perry Township is developed for mostly residential and commercial uses. Few land parcels in the area remain vacant. Encourage infill development of these lots and larger vacant parcels of land, provided the land use is compatible with existing land uses. A neighborhood plan has been adopted for the U.S. 31 and South Madison Avenue portion of this area (see Map 4). That plan contains many important recommendations for commercial and residential development in the area. Apply those recommendations to facilitate orderly development.

■ *Stage 6 (Developing Suburban Area)*

Two areas in Perry Township have been identified as Stage 6 areas. The western Stage 6 area is bounded by Bluff Road, Warman Avenue, the White River, and Stage 4. The eastern Stage 6 area is bounded by Emerson Avenue, South County Line Road, Sherman Drive, Interstate 65, Thompson Road, Stage 5, and Stage 3.

Stage 6 represents the most recently developed and developing areas in the township. Although a greater percentage of its land is developed than in Stage 7 areas, Stage 6 areas in Perry Township contain large undeveloped tracts of land, especially between Bluff Road and the Stage 7 area further west.

Because much of the area in Stage 6 is served by or has access to water and sewer service, there may be development pressure on vacant land. Areas which may experience these development pressures include the Bluff Road, Emerson Avenue, and State Road 37 roadway corridors. Because of development pressure which typically characterizes Stage 6 areas, the boundary lines for recommended land uses -- particularly in designated critical areas -- are definitive and fixed.

Conserve the significant and valuable environmental features such as the aquifer, wetlands, wooded areas, steep slopes, and endangered wildlife habitat. Furthermore, require development to utilize buffering and other techniques which minimize potentially negative impacts upon nearby existing development. Development in the floodplain should be avoided. Require any development envisioned for areas such as the White River floodplain (and the floodplains of its associated tributaries) to clearly provide for the adequate preservation of the aquifer and other natural features. Any development in floodplain areas should meet the requirements of the 100 year flood protection regulations. Preserve open space wherever extensive and/or intensive development is proposed, such as industrial or office development along I-65 and I-465. Such open space preservation maintains the area's natural character, protects environmental features, and provides relief from the potentially monotonous effects of continuous urban development. Accompany new development with careful site design and planning, as well as provision of public facilities and infrastructure.

The western Stage 6 area (surrounding State Road 37) includes part of an aquifer area. The aquifer must be protected from inappropriate land uses and development intensities that might endanger the groundwater. Further development recommendations for the aquifer are included in Critical Area 2, beginning on page 24.

■ *Stage 7 (Rural Area)*

The western remainder of the township is designated as Stage 7. Less than 5% of Stage 7 land is developed, and that development is widely distributed throughout the area. Stage 7 areas in Perry Township consist primarily of rural farm land and single-family strip tract development along existing roadways.

The township's aquifer, wetlands, wooded areas (see Maps 2 and 3), historical sites, and other environmentally sensitive areas can provide valuable open space and recreational opportunities. Do not develop areas such as the White River floodplain and its associated tributaries' floodplains unless such development clearly meets the requirements of the 100 year flood protection regulations and provides for the adequate preservation of the aquifer and other natural features. Create a White River linear park and implement the other park recommendations. In addition, throughout Stage 7 areas, protect low-intensity land uses from the noise and visual impacts of more intense uses through landscaping, sign controls, transitional yards, and other buffering measures.

Control new development in Stage 7 in order to prevent circumventing of existing adequate roadway, sewer, and water systems. Require new development to utilize existing infrastructure or provide for the expansion of such systems to serve the demand created by the development.

Stage 7 includes part of an aquifer area. The aquifer must be protected from inappropriate land uses and development intensities that might endanger the groundwater. Further development recommendations for the aquifer are included in Critical Area 2, beginning on page 24.

Critical Areas

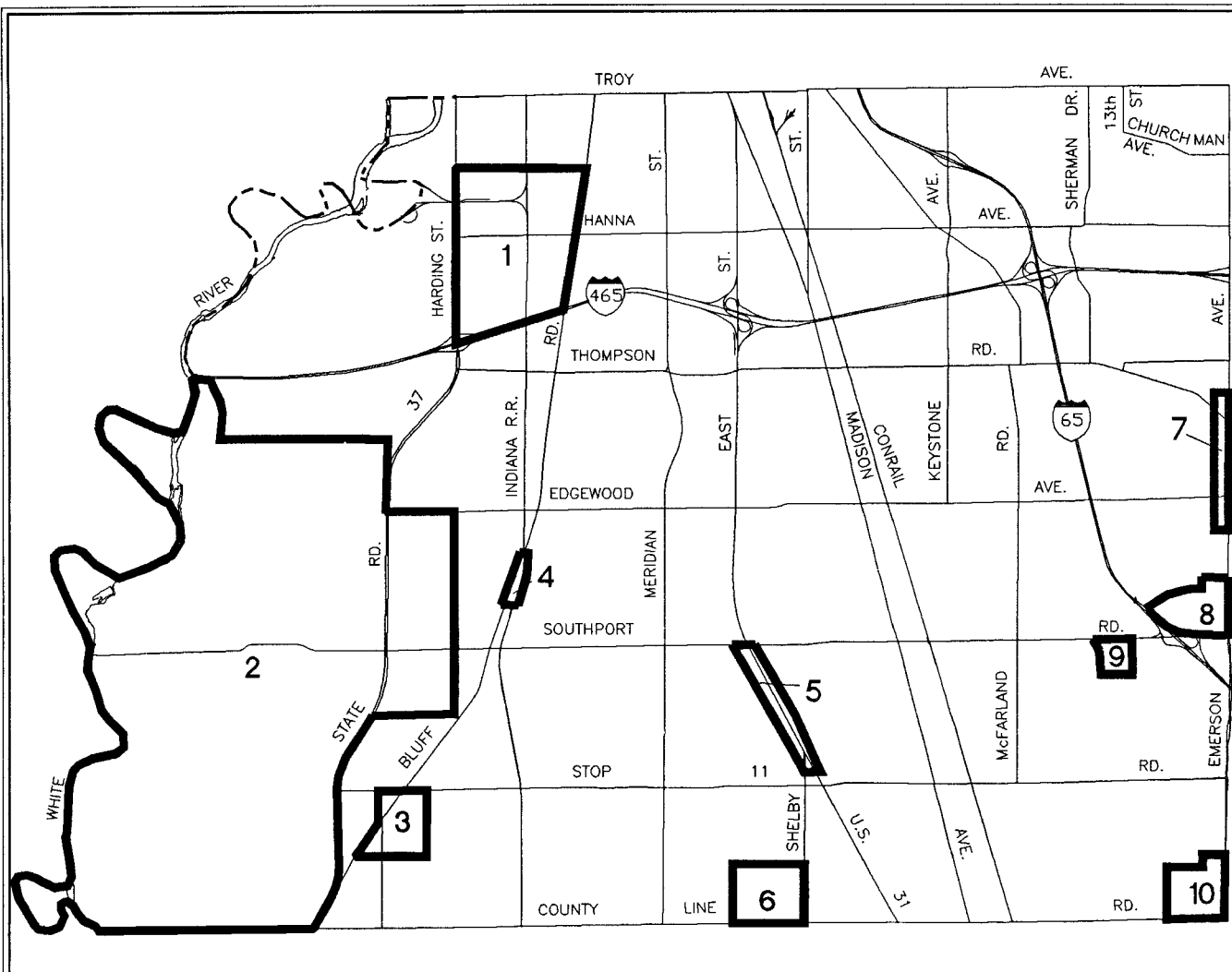
Some land use recommendations in Perry Township warrant special emphasis because of factors related to their location, unusual character, and significant development potential. It is essential to the Perry Township Comprehensive Plan that these "critical areas" be developed as recommended. The plan's land use recommendations for the remainder of the township are also important, but critical areas warrant a more detailed explanation.

This list of critical areas can only be expanded by the adoption of a new critical area as a Comprehensive Plan Segment. This process involves further study of an area by the Department of Metropolitan Development, which may then recommend an area to the Metropolitan Development Commission for adoption as a Comprehensive Plan Segment.

The following are Perry Township's critical areas, including a description and map of each, the rationale for why it is designated "critical" to the plan, recommendations for

development, and additional data (for all critical areas, see Map 8). Floodplains, wetlands, proposed Thoroughfare Plan roadway improvements, and other features are also shown on the individual critical area maps.

As stated in the introduction, this *Perry Township Comprehensive Land Use Plan* narrative is intended to be used with the *Marion County Comprehensive Plan* narrative, the *Marion County Thoroughfare Plan*, and the Perry Township Comprehensive Land Use Plan Map. The concepts contained in the *Marion County Comprehensive Plan* include land use categories, indexing, and policies for development. They apply throughout the county and are more fully explained in the *Marion County Comprehensive Plan*. Likewise, the *Marion County Thoroughfare Plan* contains more detailed information regarding roadways in Marion County. Consult these plans if necessary when using this Perry Township narrative.



MAP 8
PERRY TOWNSHIP
CRITICAL AREAS

[Thick black line symbol] **CRITICAL AREA BOUNDARIES**

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Critical Area 1

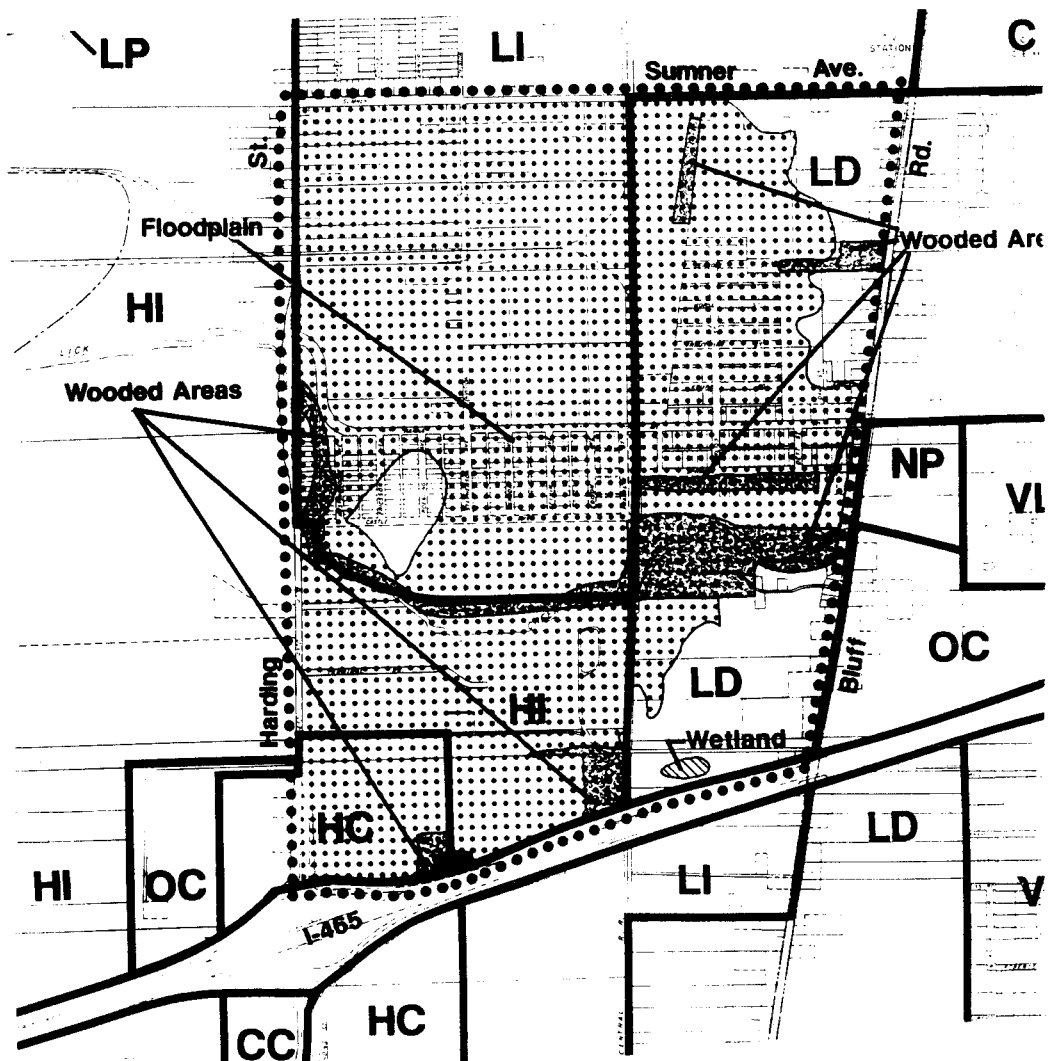
- **Location:** North of Interstate 465, west of Bluff Road, south of Sumner Avenue, and east of Harding Street.
- **Land Use Plan Recommendation:** Low Density Residential, Light Industrial, Heavy Industrial, and Heavy Commercial.
- **Surrounding Land Uses Recommended in the Comprehensive Plan:**
 - North:** Light Industrial - LI.
 - East:** Low Density Residential - LD, Neighborhood Park (Existing) - NP(E), and Office Center - OC.
 - South:** Light Industrial - LI and Heavy Commercial - HC.
 - West:** Heavy Commercial - HC, Office Center - OC, and Heavy Industrial - HI.
- **Stage of Development:** Stage 4 (Suburban Revitalization Area).
- **Why Critical:** Existing and planned residential areas need to be buffered and protected from the existing and planned industrial and commercial land uses. Thoroughfare Plan right-of-way must be obtained for proposed roadway widening of Harding Street. The area contains environmentally sensitive features including floodplain, Lick Creek, and wooded areas.
- **Recommendations:**
 - a. The boundary between industrial and residential land uses is definitive and fixed. Prevent encroachment of industrial uses into planned and existing non-industrial areas.

- b. Establish and maintain the Illinois Central Railroad as the boundary between higher intensity industrial and lower intensity residential land uses.
 - c. Buffer residential areas from industrial and commercial development through application of zoning district development standards. Critically evaluate requests for variance of development standards with an emphasis on providing the maximum protection for residential use from industrial use impacts.
 - d. Acquire additional Bluff Road and Harding Street rights-of-way by dedication.
 - e. Preserve wetlands and wooded areas as open space. Require the developer to prepare tree inventory and preservation plans. Adhere to those plans during development. In order to maintain wetlands, follow and enforce applicable federal, state, and local regulations.
 - f. This critical area contains large areas of floodplain. Development in the floodplain should be avoided. However, if development occurs in this area, it must accommodate the environmental constraints associated with floodplains by adhering to the requirements in the Flood Control Districts Zoning Ordinance administered by the Departments of Metropolitan Development and Public Works, and the Flood Control Ordinances administered by the Department of Public Works.
- Additional Data:
- a. Soil Limitations - The area's soils are primarily Urban Land-Fox-Ockley and Miami-Crosby. These soils are poorly drained. The soil associations also are unable to accommodate septic sewer systems.
 - b. Drainage - Due to the soils and flat topography, the area's drainage needs may not be met. When development occurs, require a thorough analysis of on- and off-site drainage impacts. Require drainage plans that follow the requirements contained in the City Drainage and Sediment Control Ordinance.
 - c. Environmental Constraints - Portions of the critical area are wooded. When these wooded areas are considered for development, require the developer to prepare tree inventory and preservation plans. Preserve wetland areas and incorporate them into development plans prepared by the developer.
 - d. Sanitary Sewers - The area is served by the Lick Creek Interceptor and the South and Southwest Marion County Regional Interceptor. Provide sewer services to new and existing development to prevent health hazards due to the soil associations' inability to accommodate septic systems. Prior to

development, contact the Department of Public Works' sewer permitting division regarding sewer service needs.

- e. Water Facilities - Water lines are located along Hanna Avenue and Harding Street.
- f. Transportation - The Thoroughfare Plan recommends that the segment of Harding Street from Troy Avenue to Interstate 465 be a four-lane primary arterial.

Map 9 - Critical Area 1



Critical Area 2

- **Location:** South of Interstate 465, south of Thompson Road, west of State Road 37, south of Edgewood Avenue, west of Harding Street, north of McGregor Road, west of State Road 37, north of South County Line Road, and east of the Decatur/Perry Township boundary.
 - **Land Use Plan Recommendation:** Urban Conservation indexed to Very Low Density Residential, Proposed Community Park indexed to Very Low Density Residential, Low Density Residential, Medium Density Residential, Proposed Linear Park, and Urban Conservation indexed to Medium Density Residential.
 - **Surrounding Land Uses Recommended in the Comprehensive Plan:**
 - North:** Interstate 465, and
Very Low Density Residential - VLD.
 - East:** Light Industrial - LI,
Low Density Residential - LD, and
Very Low Density Residential - VLD.
 - South:** Johnson County Comprehensive Land Use Plan indicates "Agricultural, Floodplains, and Low Density Residential" land uses.
 - West:** Urban Conservation indexed to Very Low Density Residential - UC(1),
Linear Park (Proposed) - LP(P),
Regional Park (Proposed) indexed to Very Low Density Residential - LP(P)(1),
Regional Park (Existing) - RP(E), and
Regional Park (Proposed) - RP(P).*
- * All recommended by the 1991 Decatur Township Comprehensive Plan.
- **Stage of Development:** Stage 7 (Rural Area).
- **Why Critical:** An extensive aquifer exists beneath the entire critical area, which is being developed with well fields by the Indianapolis Water Company. The aquifer extends beyond the boundaries of this critical area. Although this area is noted as

needing protection, the aquifer area outside of this critical area should also be protected. The future drinking water needs of the metropolitan Indianapolis community must be safeguarded from pollution and contamination.

There are large areas of wetlands and wooded areas which need to be preserved, and much of the area is in a floodplain which further limits development potential.

Southport Road is a primary arterial which intersects State Road 37, an expressway. Preliminary information for proposed Interstate Highway 69 indicates that its alignment could follow the alignment of State Road 37. In the future, that interstate corridor and particularly the intersection at Southport Road might appear to be viable for commercial office or retail development. However, the plan does not support commercial uses along this corridor. To assure maximum protection of the aquifer and its associated well fields, lower intensity land uses as recommended by the land use plan are more appropriate in this area.

■ Recommendations:

- a. Extend sewer and water service to all existing and proposed development in the area.
- b. Acquire Stop 11 Road right-of-way by dedication. Preserve the environmental features near its proposed right-of-way alignment.
- c. Divert chemical and hazardous waste hauling away from the area to prevent accidents and ground water pollution. Vehicles carrying hazardous waste should be routed to Interstate 65 and/or State Road 67 instead of State Road 37.
- d. Create a linear park along the White River and implement Community Park recommendations as shown on the plan map.
- e. This critical area contains large areas of floodplain. Avoid development in floodplains. Otherwise, any development approved for floodplain areas should meet the requirements of the 100 year flood protection regulations, including the Flood Control Districts Zoning Ordinance.
- f. The land use boundary between light industry and other land uses in the northern portion of the critical area is definitive and fixed. Encroachment of higher intensity uses into the lower intensity areas planned for urban conservation and residential uses is inappropriate.

- g. Recommended land uses in the critical area do not include commercial uses. Prohibit car washes, petroleum and chemical product storage, road salt storage, and other potential sources of ground water contamination. The storage and regular use of chemicals on site make such businesses potentially harmful to the drinking water supply. All development petitions must be evaluated carefully by staff regarding the use and/or storage of solvents, and other chemicals. Development petitions must include appropriate commitments or conditions that prohibit unacceptable uses. The Indianapolis Water Company must also be consulted during the development review process.
- h. Educate property owners and lease holders to prevent ground water contamination through proper management of the application of chemicals on agricultural, residential, golf course, or park land. The Indianapolis Water Company, Marion County Health and Hospital Corporation, and the Department of Metropolitan Development can conduct public education seminars, advertising, or neighborhood meetings to encourage conservation and protection of the aquifer. Recreational facilities as well as residential areas could use organic farming (the practice of using only organic materials on plant matter and natural predators to control insects); and "Tox Away Day" programs would encourage residents to safely dispose of toxic household wastes.
- i. Regulate the commercial removal of top soil and impervious clay. Restore the aquifer's overlying soil structure to areas that have been altered. In addition, restore the overlying soil structure to areas which are altered due to land development and construction activities.
- j. Regulate aggregate extraction (gravel, sand and borrow) activities within the aquifer protection area to prevent aquifer contamination.
- k. Rezone all areas for which zoning is inconsistent with the plan recommendations to appropriate classifications according to the land use plan map. Of particular concern are those areas which are zoned secondarily for gravel, sand, borrow uses.
- l. Require developers to prepare and implement plans of operation which recognize and protect the aquifer. Include in the plans of operation guidelines for ground water contamination prevention, abatement, and soil structure restoration.
- m. Implement cluster development in order to preserve wooded areas, wetlands and soil structure which protects the aquifer. (Also preserve wetlands and wooded areas as open space as indicated in n, below.) Sandy soils permit rapid percolation of contaminants to the aquifer and offer little opportunity for

mitigation of pollution. Therefore, those areas should be preserved as open space. However, clay soils are more appropriate for development. The clay soil layer acts as a buffer and impedes percolation of contaminants through the soil to the aquifer. Development on soils which are made up of clay is more appropriate than on soils which do not contain clay.

Survey and classify the soils more accurately to define the specific areas which should remain open space with no development, and those areas which are appropriate for limited development.

- n. Preserve wetland and wooded areas as open space. Utilize or create a foundation or other type of organization to obtain parts of the area for development as a park to ensure that valuable natural resources are preserved. Alternative members of the foundations may include the Indiana State Department of Natural Resources, the Indianapolis Parks Foundation, White River Greenway Development Board, White River Greenway Foundation Inc.; city, county, and state government agencies; Indianapolis Water Company; affected property owners and area residents; and environmental preservation organizations such as Friends of the White River and Groups Advocating Urban Greenspace Environments (GAUGE).
- o. Protect the groundwater quality by keeping development densities low. Preserve as much natural open space as possible. Where Urban Conservation and Park uses are indexed to Very Low Density Residential development, limit gross residential density to well below two units per acre. If development takes place, preserve wooded areas by requiring cluster development with tree inventory and preservation plans prepared by the developer.
- p. Alternatively, establish a foundation (as described in n, above) to purchase all of the land in the critical area for conservation as a wildlife refuge. This would offer the highest degree of protection for the aquifer and its associated well fields and for the wetlands and wooded areas.
- q. Inventory past, present, and potential sources of groundwater contamination and develop a contamination contingency plan.
- r. Post signs along State Road 37 and Southport Road which give emergency phone numbers and general information to motorists to facilitate immediate notification of fire departments and other emergency personnel about potential ground water contamination.
- s. Closely monitor development in the area to minimize the potential for ground water pollution. Ground water monitoring can ensure implementation of the

comprehensive plan's land use recommendations for conservation and low intensity development of the area.

- t. Develop an "Aquifer and Well Field Protection Plan" which would provide and summarize existing protection programs, map primary and secondary well field protection areas, and recommend detailed land use and development policies. This plan could include an inventory of past, present, and potential sources of groundwater contamination and a contamination contingency plan (see recommendation q.). The Marion County Health Department is an appropriate agency to undertake the plan.
- u. Study the possibility of introducing an "Aquifer Overlay Zoning District" which could specify appropriate land uses, prohibited substances, and other viable regulatory measures. These measures may include but are not limited to the following: prohibition of certain land uses, special and conditional use permits; large lot zoning, transfer of development rights, performance standards, site plan review, drainage standards, impervious coverage standards, chemical and fuel storage controls, sewage discharge permits, waste water treatment plant controls, regulation of septic system cleaners, septic system conversion to sewer systems, toxic and hazardous materials prohibitions, and nitrogen loading standards.
- v. Prohibit dry wells in this critical area. Most drainage measures allow storm water to slowly percolate through the soil layers, be cleansed of some contaminants and eventually reach the aquifer. Dry wells, on the other hand, facilitate site drainage by bypassing layers of soil and directly expose the aquifer to contaminants.

■ Additional Data:

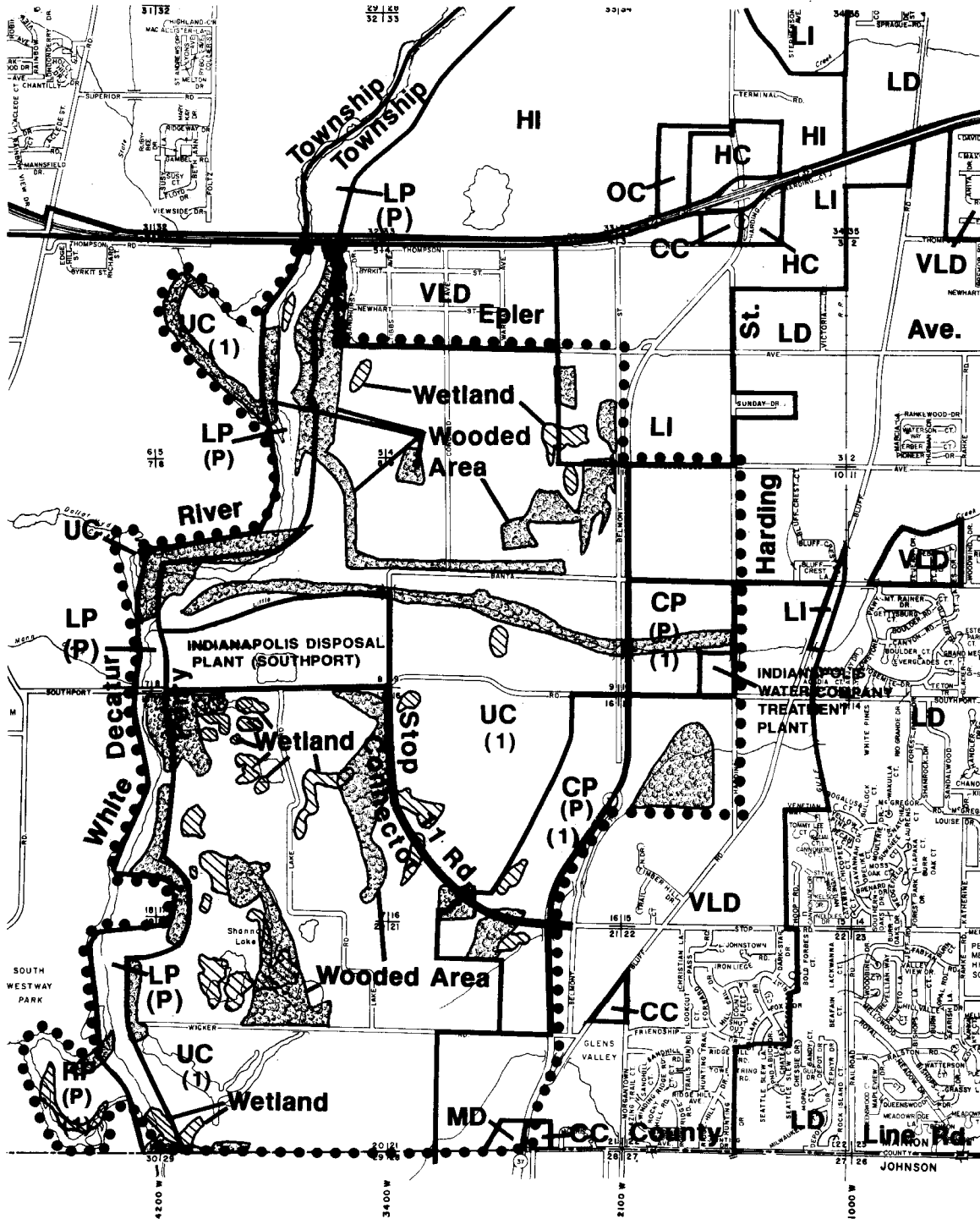
- a. Soil Limitations - The site's soils are primarily Genesee-Sloan association and Urban Land-Fox-Ockley. These soils are characterized as well drained and somewhat poorly drained. Since the topography is nearly level, the area is generally very poorly drained.
- b. Drainage - Drainage problems associated with the area's soils and flat topography may not be met. Thorough drainage analysis and planning is necessary. Development according to the requirements contained in the City Drainage and Sediment Control Ordinance may accommodate some drainage problems.
- c. Environmental Constraints - Protect the aquifer through strict control of land use. Portions of the critical area are wooded or wetlands -- preserve these areas in development plans.

Aggregate mining (see page 8), top soil removal, and/or other excavation activity would likely destroy wooded areas and wetlands. These operations could also expose the aquifer, thus jeopardizing the metropolitan Indianapolis water supply. In addition, the use of pesticides, herbicides, fertilizers, and the disposal of household wastes (e.g. automobile oil, cleaning products, paint, and other wastes) must be controlled within the critical area to prevent contamination of the aquifer. In order to minimize the potential for accidental spills and contamination of the aquifer, divert transportation of hazardous and chemical materials from the critical area.

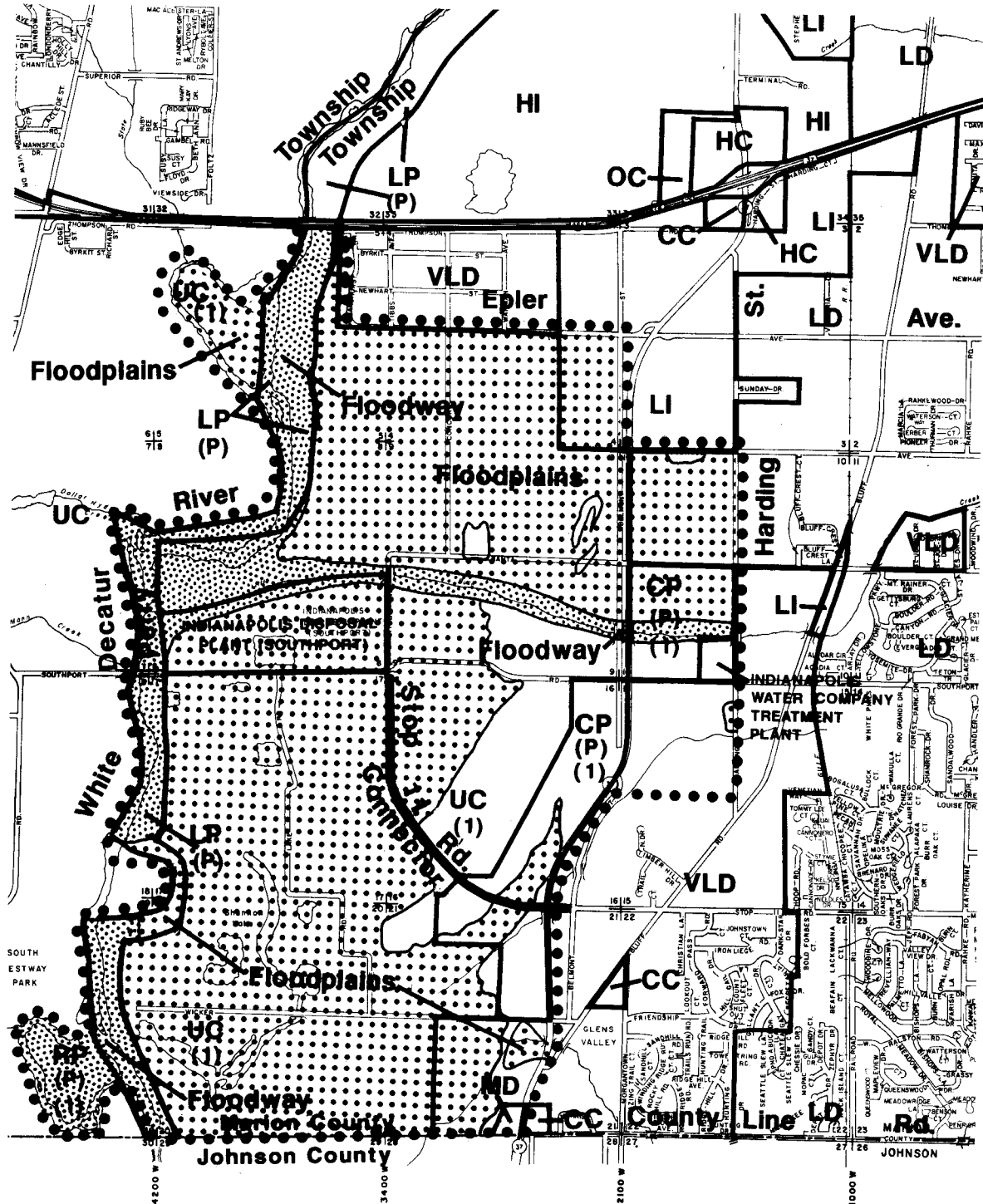
- d. Sanitary Sewers - Development in the area can be served by the existing South and Southwest Marion County Regional Interceptors. Provide sewer service to new and existing development to prevent health hazards due to the soil associations' inability to accommodate septic systems (in addition to guarding against potential aquifer contamination). The developer should consult the Department of Public Works' sewer permitting division when development occurs in this area.
- e. Water Facilities - Water service is currently available to the area from the Southport Water Main, which serves the Indianapolis Disposal Plant.
- f. Transportation - The Thoroughfare Plan recommends that Stop 11 Road from Southport Road to State Road 37 initially be constructed as a two-lane roadway until traffic demand warrants four lanes. Implementation is recommended between the years 2002 and 2005. Divert chemical and hazardous material traffic from State Road 37 in order to minimize the potential for traffic accidents involving chemical and hazardous waste carriers.

(See Map 10.a and Map 10.b, next pages)

Map 10.a - Critical Area 2
Wetlands/Wooded Areas



Map 10.b - Critical Area 2
Flood-prone Areas



Critical Area 3

- Location: North of Friendship Drive, west of Christian Lane, south of Stop 11 Road, and east of Bluff Road.
- Land Use Plan Recommendation: Commercial Cluster and Very Low Density Residential.
- Surrounding Land Uses Recommended in the Comprehensive Plan:
 - Northwest: Very Low Density Residential - VLD.
 - East: Very Low Density Residential - VLD.
 - South: Very Low Density Residential - VLD.
 - West: Very Low Density Residential - VLD.
- Stage of Development: Stage 6 (Developing Suburban Area).
- Why Critical: There are currently about five commercial uses in this critical area. These uses are concentrated in the area recommended for Commercial Cluster on the Comprehensive Land Use Plan Map. It is reasonable that these uses will continue operation in the future in any of the following ways: operate with no changes; expand individual commercial establishments; or remove existing buildings and construct new commercial buildings. There is also potential for additional commercial development outside of the Commercial Cluster area into the currently vacant land east and west of Bluff Road. Therefore, the boundary between Commercial Cluster and Very Low Density Residential land uses is critical.

This area contains a site which is proposed for development as a Community Park (see Map 11). The Indianapolis/Marion County Board of Parks and Recreation approved the property for acquisition by the Parks Department in February, 1992. Since the land use plan map was developed prior to the Board decision, the park is not shown on the Perry Township Comprehensive Land Use Plan Map. The park contains nearly one-half of the wooded area in this critical area, and extends from Bluff Road on the west, Stop 11 Road on the north, to Christian Lane (extended south) on the east. The southern boundary is adjacent to the Glens Valley Elementary School.

The Perry Township School District has decided to expand some of its existing schools. If the Glens Valley Elementary School is expanded the wooded area east of the school, as well as park land north and east of the school, could be impacted.

■ Recommendations:

- a. Interpret the boundary between commercial and residential land uses as definitive and fixed. Establish Morgantown Road as the easternmost boundary and Bluff Road as the westernmost boundary of commercial land uses. Establish the southern boundary of the commercial area as shown on the plan map. Prevent encroachment of commercial uses into planned and existing residential land uses.
- b. Provide buffering of commercial development by landscape treatment of boundaries adjacent to developed and planned residential land uses.
- c. Preserve the wooded area in the southeast corner of the critical area as open space. If that area is developed for residential purposes, a cluster subdivision should be utilized. Require the developer to prepare tree inventory and preservation plans.
- d. Prepare a park master plan and develop the park.
- e. Prohibit encroachment of all development into the park area.

■ Additional Data:

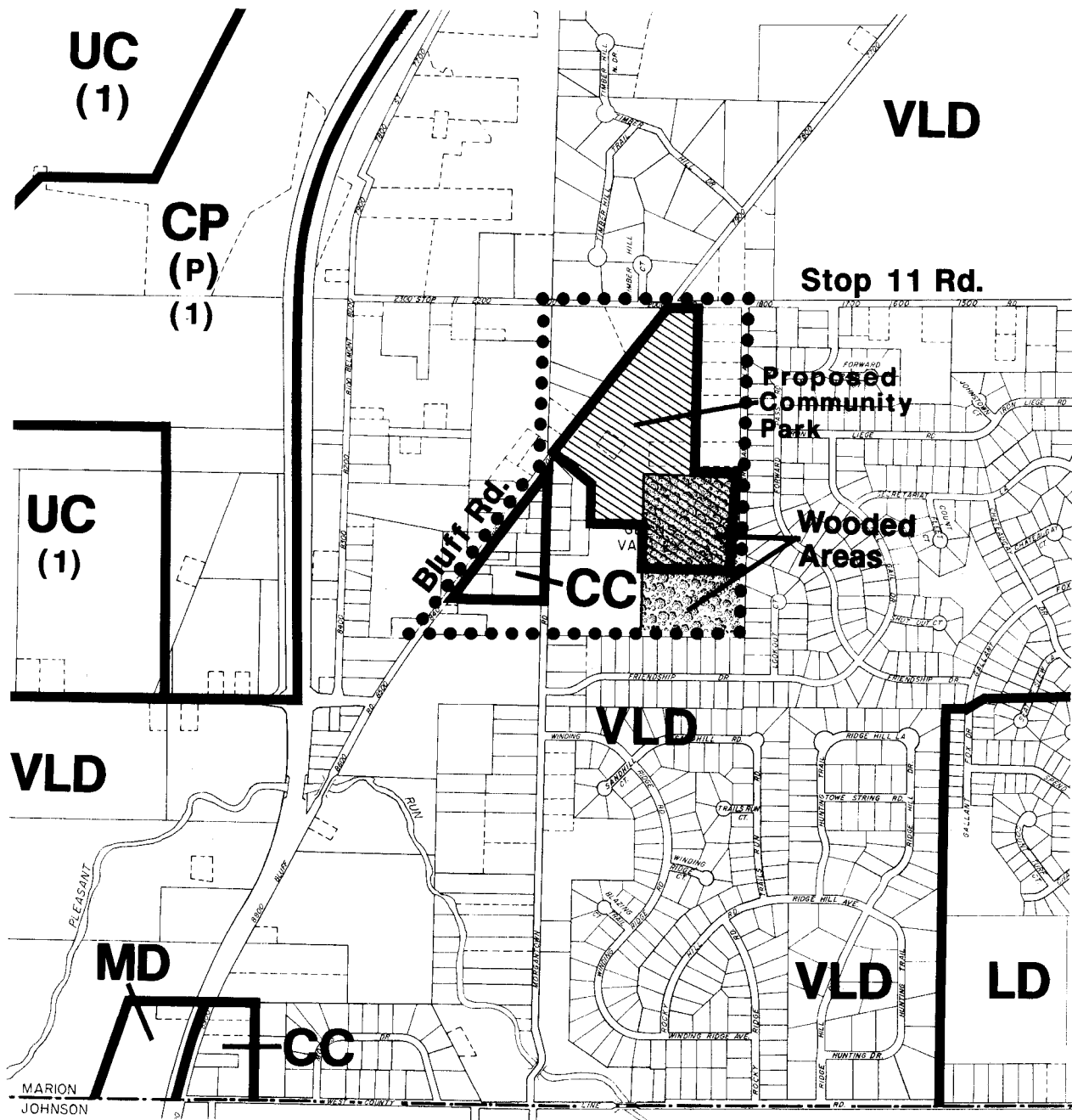
- a. Soil Limitations - The site soil association is Urban Land-Fox-Ockley. This soil is characterized as well drained, nearly level to moderately sloping. Although these soils are more accommodating of septic systems, than other soil associations in Perry Township, provide sewer services to new and existing development to prevent health hazards.
- b. Drainage - Although the soil associations in this critical area are well drained, there may be isolated areas with drainage problems. In such areas, proper drainage can be accommodated through appropriate site grading, design, and off-site improvements as required by the City Drainage and Sediment Control Ordinance.
- c. Environmental Constraints - The southeast corner of the critical area is wooded. When that wooded area is considered for development, prepare tree inventory and preservation plans and incorporate them into development plans.

Implement cluster subdivision development for maximum preservation of the wooded area.

- d. Sanitary Sewers - The area is not served by sanitary sewers. Areas east and south of the critical area are served by sanitary sewers. When development occurs, extend sanitary sewer service into the area. Consult with the Department of Public Works' sewer permitting division when considering sewer improvements.
- e. Water Facilities - Water service is available to the area from water lines which currently serve developed subdivisions east and south of this critical area.
- f. Transportation - The area contains one primary arterial roadway: Bluff Road and two secondary arterial roadways: Stop 11 Road and Morgantown Road. No new arterial streets or priority improvements are recommended by the *Marion County Thoroughfare Plan* for the area.

(See Map 11, next page)

Map 11 - Critical Area 3



Critical Area 4

- **Location:** North of Buck Creek, west of Illinois Central Railroad, and east of Bluff Road.
- **Land Use Plan Recommendation:** Light Industrial.
- **Surrounding Land Uses Recommended in the Comprehensive Plan:**
 - North:** Low Density Residential - LD.
 - East:** Low Density Residential - LD.
 - South:** Low Density Residential - LD, and
Very Low Density Residential - VLD.
 - West:** Low Density Residential - LD, and
Very Low Density Residential - VLD.
- **Stage of Development:** Stage 5 (Established Suburban Area).
- **Why Critical:** The entire boundary of industrial land use is critical because of the adjacent residential recommendation and associated potential for encroachment of that use into the existing and planned residential development. In 1962 the critical area was mostly vacant with a few residential structures. Nearly one-half of the critical area was still vacant by 1974. In 1979, much of the critical area was built up. Existing land uses in the area include a single family residence, roller skating rink, lumber yard, and light industrial uses. Since the area is fully developed, there may be pressure from the development community to expand the industrial area south of Buck Creek, west of Bluff Road, or north of the critical area boundary.

The critical area is also in close proximity to the Indianapolis Water Company well fields, is within the boundaries of the aquifer, and is in a floodplain. Industrial uses in the critical area could result in industrial accidents and contamination of the future metropolitan Indianapolis water resource.

■ Recommendations:

- a. Eliminate outdoor trash disposal and haul away all wastes for disposal (rather than on-site disposal). These steps can reduce the potential for accidental ground water contamination.
- b. Interpret the boundary between industrial and residential land uses as definitive and fixed. Prevent the expansion and encroachment of industrial uses into planned and existing non-industrial areas. Also, the intensity of the industrial uses must have minimal impact upon the surrounding area.
- c. Provide buffering of industrial development adjacent to residential planned and developed areas as required by zoning district development standards.
- d. Develop the area with sensitivity to the environmental features and constraints associated with floodplains according to the requirements of the Flood Control Districts Zoning Ordinance.
- e. Limit industrial uses to those posing no threat to the Perry Township aquifer and its associated public well fields.
- f. Storage, processing, or distribution of petrochemicals, salts, solvents, detergents, or other chemicals is not appropriate.
- g. Consult the Indianapolis Water Company regarding any development petitions.

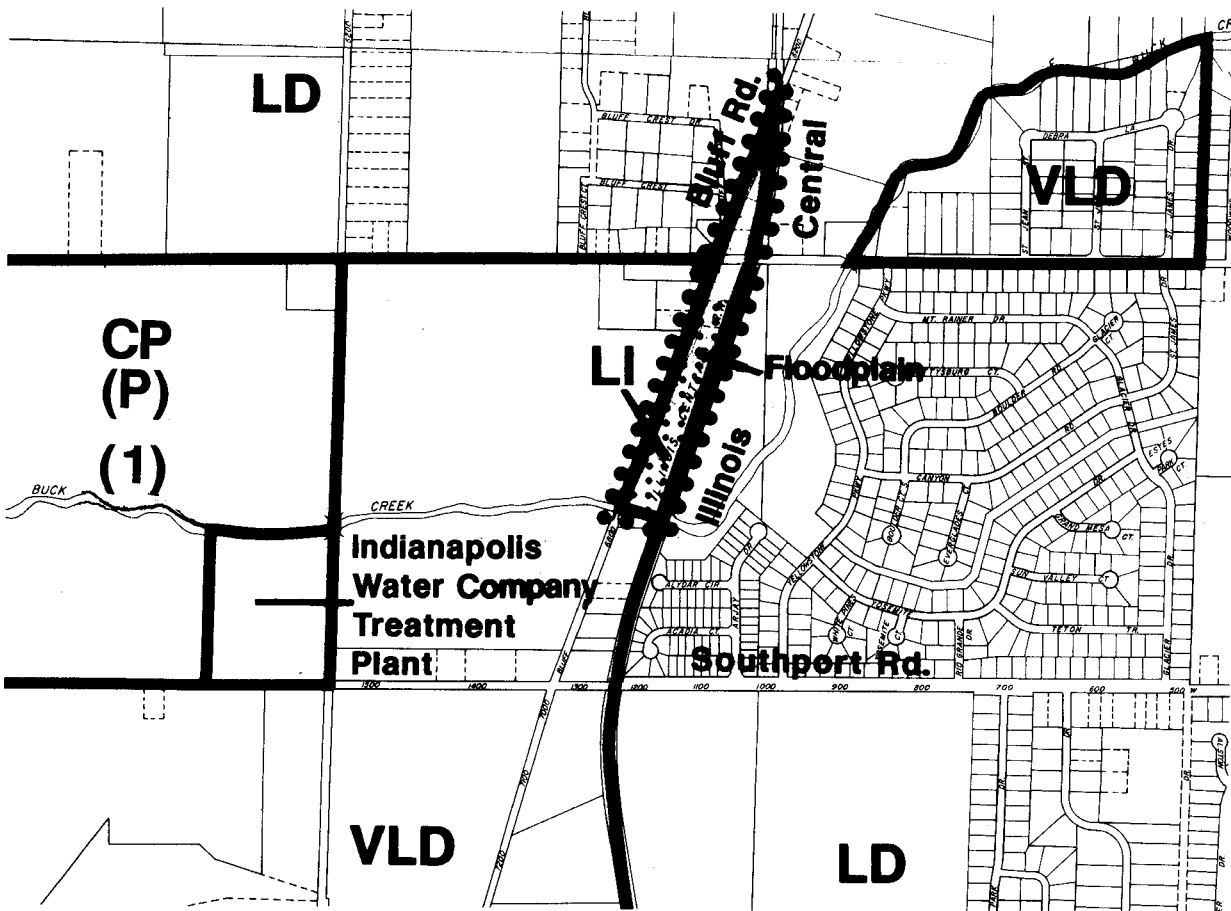
■ Additional Data:

- a. Soil Limitations - The site's soils are Genesee-Sloan and Miami-Crosby. These soils are characterized as well drained and somewhat poorly to very poorly drained. The soil associations are also characterized as unable to accommodate septic systems.
- b. Drainage - Drainage problems associated with the area's soils flat topography may not be met. Thoroughly consider drainage needs including on- and off-site improvements. The City Drainage and Sediment Control Ordinance contains development requirements which must be met.
- c. Environmental Constraints - Although much of the area is already developed, new development should avoid the floodplain. Otherwise, develop the site with sensitivity to the floodplain and Buck Creek. Minimize the potential for groundwater pollution as a result of storm water runoff or chemical spills by

using water runoff retention systems and by restricting industrial activities to those which are least likely to use hazardous chemicals, solvents, or heavy metals.

- d. **Sanitary Sewers** - The area is not currently served by sanitary sewers. Sanitary sewer lines are located one-quarter of a mile east of the Illinois Central Railroad. Provide sewer services to new and existing development to prevent health hazards due to the soil associations' inability to accommodate septic systems. Contact the Department of Public Works' sewer permitting division for direction when sewer service is considered.
- e. **Water Facilities** - The area is not currently served by water service. Water service exists and is available to the area from water lines east of the Illinois Central Railroad and north of Edgewood Avenue.
- f. **Transportation** - The Thoroughfare Plan recommends no new roadways or improvements in this area.

Map 12 - Critical Area 4



Critical Area 5

- **Location:** South of Southport Road, west of Camden Street, north of Stop 11 Road, and east of Surrey Road.
- **Land Use Plan Recommendation:** Office Buffer, Neighborhood Shopping Center, Community Shopping Center, Commercial Cluster, and Low Density Residential.
- **Surrounding Land Uses Recommended in the Comprehensive Plan:**
 - North:** Office Buffer - OB.
 - East:** Commercial Cluster - CC,
Low Density Residential - LD,
Community Shopping Center - CSC, and
Neighborhood Shopping Center - NSC.
 - South:** Neighborhood Shopping Center - NSC,
Commercial Cluster - CC, and
Public Use indexed to Medium Density Residential - Public Use (3).
 - West:** Very Low Density Residential - VLD.
- **Stage of Development:** Stage 5 (Established Suburban Area).
- **Why Critical:** The established residential neighborhoods adjacent to the planned office and commercial areas along U.S. 31 South need protection from the impact and encroachment of commercial retail and commercial office development.
- **Recommendations:**
 - a. Maintain required setbacks, sign regulations, landscaping, and other development standards.
 - b. Limit commercial uses to the areas designated by the plan map to prevent encroachment along collector roadways that intersect with U.S. 31 and into established single-family residential areas. Limit commercial development to parcels having frontage and access to U.S. 31 South.

- c. Integrate commercial development with combined access drives, shared parking, and integrated sign programs.

■ Additional Data:

- a. Soil Limitations - The site's soils are Urban Land-Fox-Ockley and Miami-Crosby. These soils are characterized as well drained and somewhat poorly drained, respectively. The topography ranges from nearly level to moderately steep soils. Provide sewer services to new and existing development to prevent health hazards due to the soil associations' inability to accommodate septic systems.
- b. Drainage - Drainage problems associated with the Miami-Crosby soils and the site's flat topography may not be met. When development occurs thoroughly analyze drainage needs on- and off-site. Make improvements as determined necessary and as required by the City Drainage and Sediment Control Ordinance.
- c. Environmental Constraints - There are no unique environmental constraints within the boundaries of this critical area.
- d. Sanitary Sewers - Most of the southern half of this critical area is served by sanitary sewers. The remaining half is not served by sanitary sewers. Extend sewer lines north and south of the critical area to serve the northern portion of the area. Consult with the Department of Public Works' sewer permitting division for direction when sewer service is considered.
- e. Water Facilities - Water service exists and is available to the area.
- f. Transportation - The Thoroughfare Plan recommends no new roadways or major improvements in this area.

(See Map 13, next page)

Critical Area 6

- Location: North of South County Line Road, west of Shelby Street, south of Logwood Drive, and east of East Street.
- Land Use Plan Recommendation: Medium Density Residential and Low Density Residential.
- Surrounding Land Uses Recommended in the Comprehensive Plan:
 - North: Low Density Residential - LD, and Medium Density Residential - MD.
 - East: Hospital - H.
 - South: The Greenwood Comprehensive Plan recommends "Medium Low Density Residential and Institutional" land uses.
 - West: Medium Density Residential - MD, and Low Density Residential - LD.
- Stage of Development: Stage 5 (Established Suburban Area).
- Why Critical: The site contains wooded areas, floodplain, and wetlands which need to be preserved. The boundary between Low Density Residential and Medium Density Residential could experience adverse development pressure, resulting in a larger than planned medium density development and a burden on infrastructure and community services.
- Recommendations:
 - a. Interpret the boundary between Low Density Residential and Medium Density Residential as definitive and fixed.
 - b. Through preservation of wooded areas, a natural boundary can be created between the Medium Density and Low Density Residential areas. Require developer prepared tree inventory and preservation plans when development takes place.

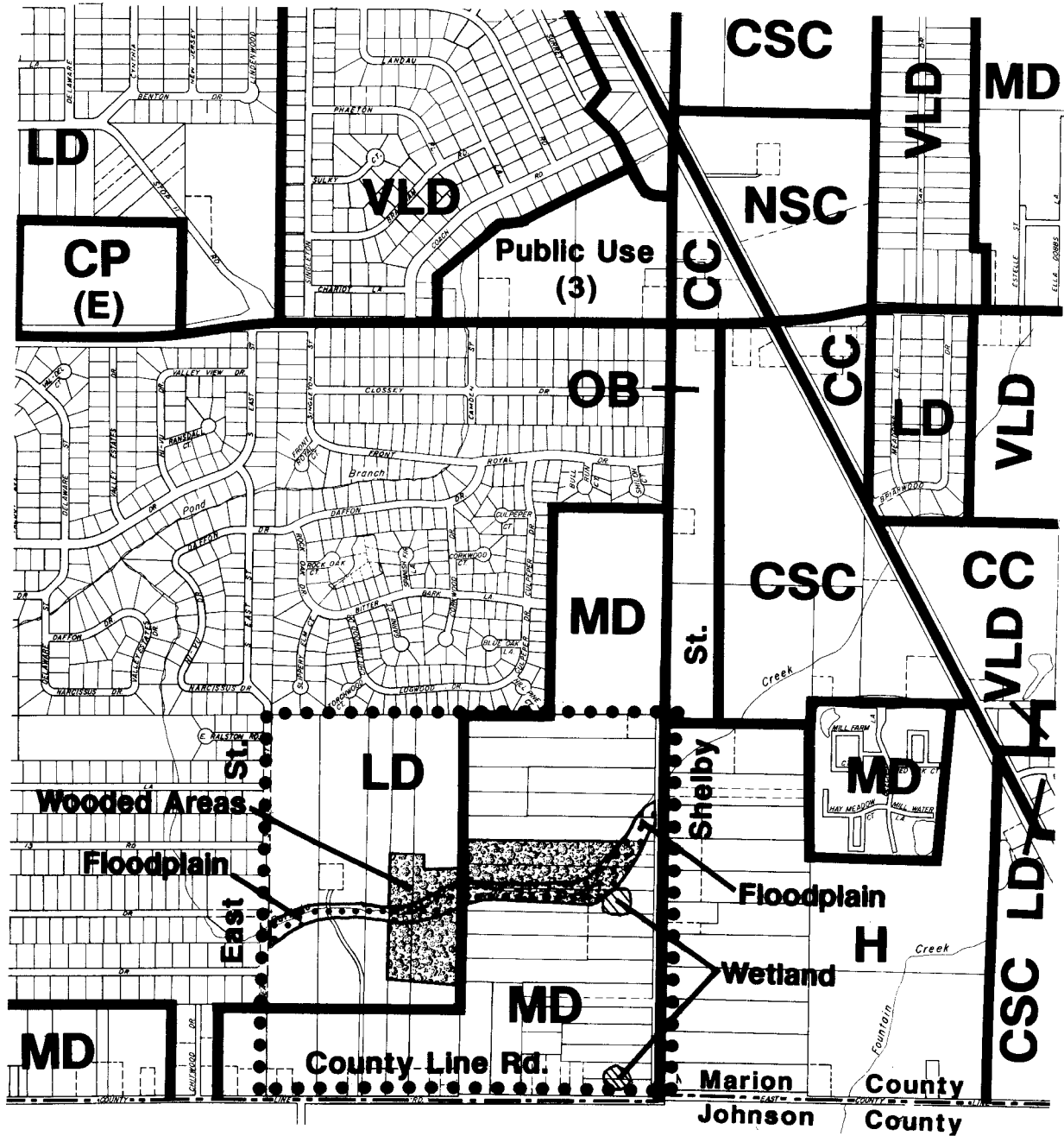
- c. Protect and preserve wetland areas prior to, during, and after development occurs. Appropriate uses of the wetlands include but are not limited to parks, nature areas, and biology study areas for schools.
- d. This critical area contains areas of floodplain. Development in floodplain areas should be avoided. However, any development approved in the floodplain should meet the requirements of the 100 year flood protection regulations, including the Flood Control Districts Zoning Ordinance.
- e. Redevelop the long, narrow residential lots off of Shelby Street for medium density residential uses. Integrate these lots when redevelopment takes place.

■ Additional Data:

- a. Soil Limitations - The site's principle soil association is Crosby-Brookston. This soil association is characterized as somewhat poorly drained and very poorly drained. The topography is nearly level to gently sloping. Due to these natural constraints, drainage needs may not be met.
- b. Drainage - When development occurs, drainage should be thoroughly analyzed to determine specific drainage needs. Drainage problems associated with the Crosby-Brookston soils may be accommodated through appropriate site grading, design, and off-site improvements as required by the City Drainage and Sediment Control Ordinance.
- c. Environmental Constraints - Portions of the critical area are wooded. When these wooded areas are considered for development, prepare tree inventory and preservation plans and incorporate them into development plans. Preserve wetland areas.
- d. Sanitary Sewers - The area is served by the South Marion County Interceptor. Provide sewer services to new and existing development to prevent health hazards due to the soil associations' inability to accommodate septic systems. The Department of Public Works' sewer permitting division should be consulted when sewer service improvements are considered.
- e. Water Facilities - Water service exists and is available to the area from water lines north and east of the critical area.
- f. Transportation - The Thoroughfare Plan recommends no new roadways or priority improvements in the area.

(See Map 14, next page)

Map 14 - Critical Area 6



Critical Area 7

- Location: North and south of Shelbyville Road, west of Emerson Avenue, one-quarter mile south of Edgewood Avenue, east of a line parallel to and one-eighth mile west of Emerson Avenue.
- Land Use Plan Recommendation: Low Density Residential.
- Surrounding Land Uses Recommended in the Comprehensive Plan:
 - North: Low Density Residential - LDR.
 - East: Medium Density Residential - MD,
Commercial Cluster - CC, and
Very Low Density Residential - VLD.
 - South: Low Density Residential -LD.
 - West: Low Density Residential - LD.
- Stage of Development: Stage 6 (Developing Suburban Area).
- Why Critical: The boundary between Commercial Cluster and Very Low Density Residential land uses is critical because of the potential for encroachment of large tract or strip commercial land uses into the existing and planned residential areas. Commercial development could negatively impact the area's roadway system and public safety.
- Recommendations:
 - a. The Comprehensive Plan recommends Low Density Residential use, which is appropriate according to surrounding residential development. This recommendation is consistent with the basic principles of land use intensity and is compatible with the land use recommendation of Franklin Township, east of Emerson Avenue.
 - b. The boundary between commercial and residential land uses is fixed. Do not permit commercial uses to become established in this area because of their

negative effects on the roadway system and their incompatibility with residential uses. This portion of Emerson Avenue consists mostly of residential uses between two commercial centers. If commercial uses are allowed on one or more lots, others would probably follow due to increased pressure for commercial development.

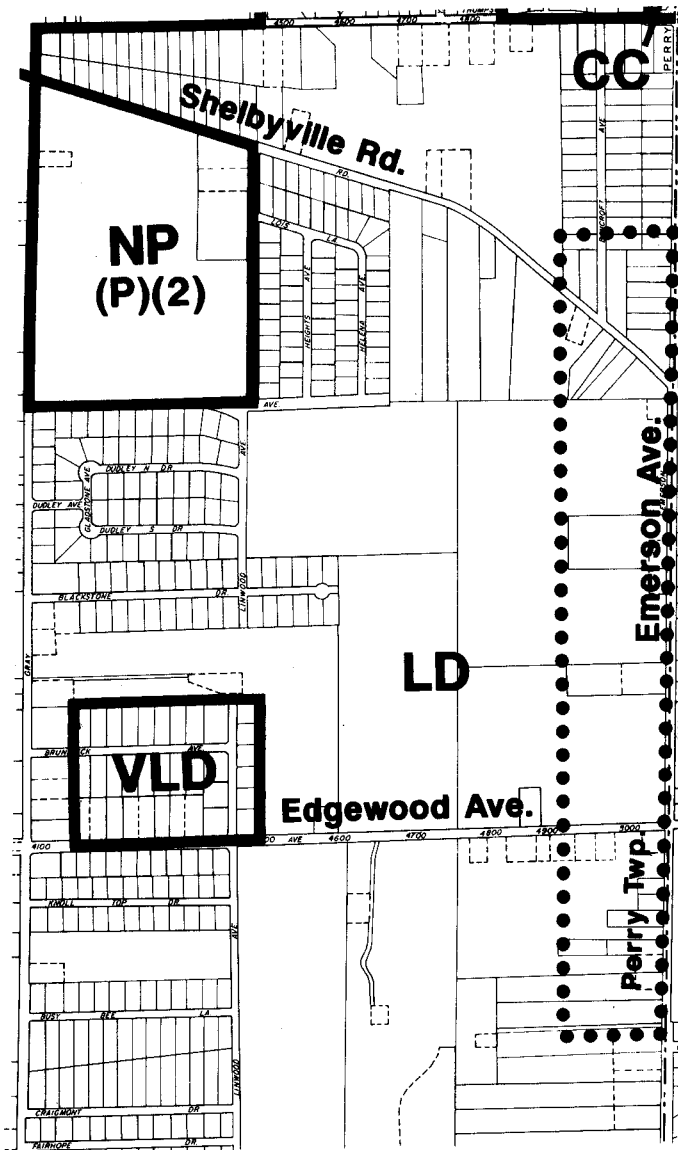
- c. Encourage commercial development at appropriate locations on Emerson Road designated for commercial uses on the Comprehensive Plan: the Emerson Avenue/Thompson Road intersection and the Emerson Avenue/Southport Road intersection.
- d. Emerson Avenue is shown as a primary arterial on the Thoroughfare Plan. Any development that occurs must include Emerson Avenue right-of-way dedication. The lot depths appear sufficient to accommodate widening of Emerson Avenue as well as provide setbacks for landscaping, fencing, and other residential/thoroughfare buffering.

■ Additional Data:

- a. Soil Limitations - The site's soil association is Crosby-Brookston. This soil association is characterized as somewhat poorly drained and very poorly drained, with slope ranging from nearly level to gently sloping soils. Provide sewer service to new and existing development to prevent health hazards due to the soil association's inability to accommodate septic systems.
- b. Drainage - The soil associations in this critical area are poorly drained. Drainage needs should be thoroughly analyzed prior to development of the area. Proper drainage should be accommodated through appropriate site grading, design, and off-site improvements as required by the City Drainage and Sediment Control Ordinance.
- c. Environmental Constraints - The Division of Planning has determined that there are no unique environmental constraints within the boundaries of this critical area.
- d. Sanitary Sewers - Sanitary sewer service exists west of the area and may be extended to existing and new development. Consult the Department of Public Works' sewer permitting division in order to fully assess and plan for sanitary sewer service.
- e. Water Facilities - Water service exists and is available to the area from water lines which extend along Emerson Avenue.

- f. Transportation - Although there is enough capacity to handle existing traffic on Emerson Avenue, this roadway probably will be at or near capacity by the year 2005. Commercial development from Shelbyville Road to Little Buck Creek would bring further congestion and safety hazards to Emerson Avenue. In addition, the Thoroughfare Plan recommends no new roadway priority improvements in the area.

Map 15 - Critical Area 7



Critical Area 8

- Location: North and east of Interstate 65, north of Southport Road, west of Emerson Avenue, and south of Buck Creek.
- Land Use Plan Recommendation: Office Center, Proposed Neighborhood Park indexed to Low Density Residential, Office Buffer, and Commercial Cluster.

- Surrounding Land Uses Recommended in the Comprehensive Plan:

North: Low Density Residential - LD.

East: Very Low Density Residential - VLD,
Proposed Linear Park indexed to Very Low Density Residential -
LP(P)(1), and
Office Center - OC.

South: Community Shopping Center - CSC.

West: Interstate 65, and
Low Density Residential - LD.

- Stage of Development: Stage 6 (Developing Suburban Area).
- Why Critical: Environmentally sensitive areas are located within the critical area boundaries and should be preserved. Development pressure could result in unsuitable land uses such as commercial in areas designated for park and residential use.

Commercial development should be an integrated *office park*. Retail development would generate more traffic than an office park. Evening and weekend activity would be significantly greater. This area is critical since the surrounding area lacks a significant office/business center and will likely require one in the future.

- Recommendations:
 - a. The boundary between commercial and residential land uses is definitive and fixed. The commercial uses must not encroach into planned and existing non-commercial areas.

- b. Provide buffering of commercial development by adhering to required setback regulations and landscaping adjacent to planned and developed residential areas as required by zoning district development standards.
- c. Integrate commercial development by shared drives, parking, an access road, and other site development measures. Integrated signing should also be used to minimize the visual impact on nearby residential areas.
- d. This critical area contains floodway, floodplains, wooded areas, and wetland areas. Development in floodplains should be avoided. However, any development permitted in the floodplains should meet the requirements of 100 year flood regulations, including the Flood Control Districts Zoning Ordinance.
- e. Acquire park land and establish park uses as recommended in the Comprehensive Plan. This area is not currently served by a park. Future residential development surrounding this critical area will require open space including parks. Implement the land use plan's recommendation for a neighborhood park in anticipation of that need.
- f. Maintain the wetland in its natural state and preserve it as open space before, during and after development of the area.
- g. Require the developer to prepare tree inventory and preservation plans so that the development complements and embraces the natural features of the area.

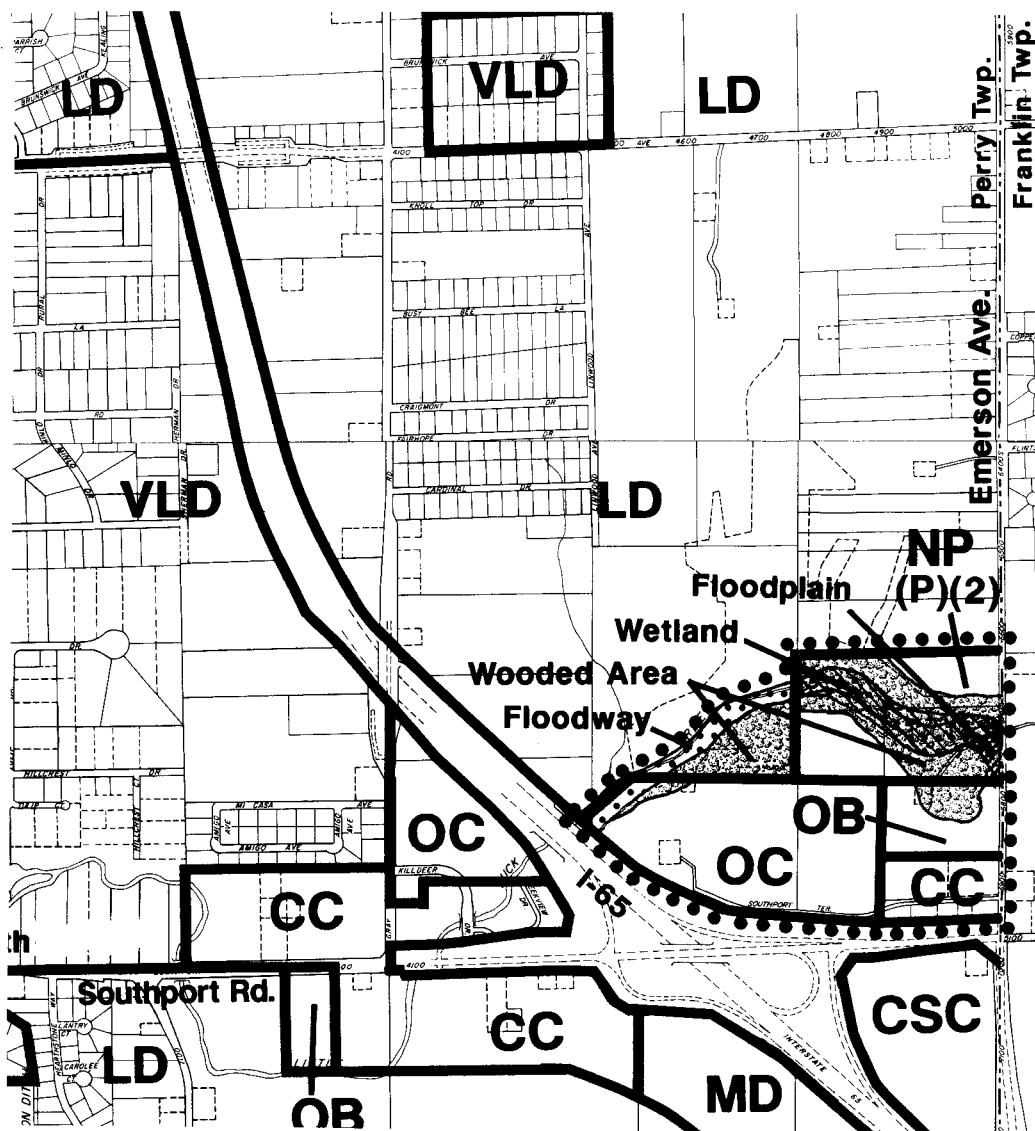
■ Additional Data:

- a. Soil Limitations - The site's soils are Genesee-Sloan. These soils are characterized as well drained and very poorly drained. Provide sewer services to new and existing development to prevent health hazards due to the soil associations' inability to accommodate septic systems.
- b. Drainage - Drainage needs may not be met when development occurs due to problems associated with the soils and the topography. Site drainage should be thoroughly analyzed and may be accommodated through appropriate site grading, design, and off-site improvements as required by the City Drainage and Sediment Control Ordinance.
- c. Environmental Constraints - The area contains floodway, floodplain, Buck Creek, wooded areas, and wetlands.
- d. Sanitary Sewers - The area can be served by the Lick Creek Interceptor. Any

proposals for sewer improvements should include consultation with the Department of Public Works' sewer permitting division.

- e. Water Facilities - Water service exists and is available to the area from water lines along Emerson Avenue and Southport Road.
- f. Transportation - There are no new arterial streets or improvements recommended by the Marion County Thoroughfare Plan for this area. When development occurs, obtain additional right-of-way should be dedicated for roadway widening of Emerson Avenue, if required.

Map 16 - Critical Area 8



Critical Area 9

- **Location:** South of Southport Road, west of Gray Road, north of Pine Top Drive, and east of Sherman Drive.
- **Land Use Plan Recommendation:** Low Density Residential, Office Buffer, and Commercial Cluster.
- **Surrounding Land Uses Recommended in the Comprehensive Plan:**
 - North:** Commercial Cluster - CC, and Very Low Density Residential - VLD.
 - East:** Commercial Cluster - CC, and Low Density Residential - LD.
 - South:** Low Density Residential - LD.
 - West:** Low Density Residential - LD.
- **Stage of Development:** Stage 5 (Established Suburban Area).
- **Why Critical:** The established and planned residential neighborhoods adjacent to the commercial office and retail areas need protection from the impact and encroachment of commercial office and retail development. The wetland and wooded areas along Little Buck Creek need preserved and utilized as a buffer between commercial and residential uses.
- **Recommendations:**
 - a. Integrate commercial development by use of shared access drives, shared parking, and other measures.
 - b. Adhere to required setbacks, sign regulations, landscaping, and other zoning ordinance development standards.
 - c. Limit commercial uses to the areas designated by the plan map (north and east of Little Buck Creek) to prevent encroachment into established single-family

residential areas.

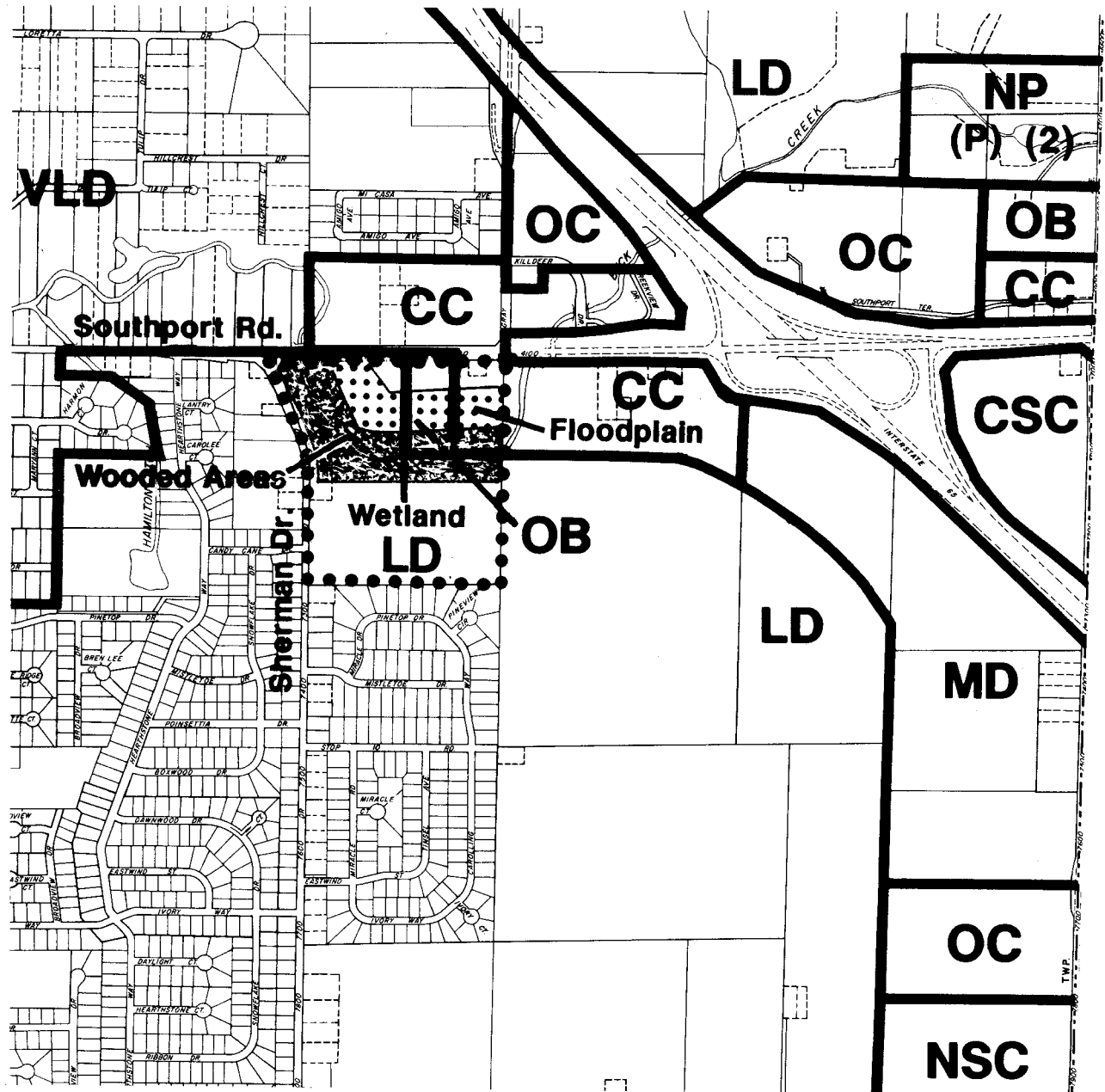
- d. Require the developer to prepare tree inventory and preservation plans at the time of development.
- e. Preserve and maintain wetlands. Alteration of the wetlands requires a permit from the Indiana Department of Environmental Management and the Army Corps of Engineers.

■ Additional Data:

- a. Soil Limitations - This site's soil is Genessee-Sloan. This soil is characterized as well drained and very poorly drained. Sewer services should be provided to new and existing development to prevent health hazards due to the soil associations' inability to accommodate septic systems.
- b. Drainage - When development occurs drainage should be analyzed since drainage (flooding) associated with the Genessee-Sloan is problematic. Over one-half of the area is comprised of floodplain. Development in the floodplain should be avoided. However, adherence to the 100 year flood protection regulations, including the Flood Control Districts Zoning Ordinance, are required if development is approved in the floodplain. In addition, development will be subject to the City Drainage and Sediment Control Ordinance.
- c. Environmental Constraints - Develop the site sensitive to the wetland, wooded areas, floodplain areas, and topography (a nearly 15 foot high bluff makes up part of the western boundary of this critical area). Preserve and incorporate in development plans as buffering between higher and lower intensity uses.
- d. Sanitary Sewers - This critical area is served by sanitary sewers. Contact the Department of Public Works' sewer permitting division when considering new sewer service.
- e. Water Facilities - Water service exists and is available to the area from water lines along Southport Road and McFarland Road.
- f. Transportation - The Thoroughfare Plan recommends no new roadways or priority improvements in this area.

(See Map 17, next page)

Map 17 - Critical Area 9



Critical Area 10

- Location: North of South County Line Road, west of Emerson Avenue, south of Ralston Road, and east of Carl Smock Golf Course.
- Land Use Plan Recommendation: Low Density Residential and Clear Zone for the Greenwood Municipal Airport.
- Surrounding Land Uses Recommended in the Comprehensive Plan:
 - North: Low Density Residential - LD.
 - East: Low Density Residential - VLD, and Office Buffer - OB.
 - South: The City of Greenwood land use plan recommends "Industrial and Airport Transition Uses".
 - West: Low Density Residential - LD.
- Stage of Development: Stage 6 (Developing Suburban Area).
- Why Critical: The clear zone (area at the end of an airport runway) for the Greenwood Municipal Airport extends over South County Line Road approximately one-eighth mile into the critical area.

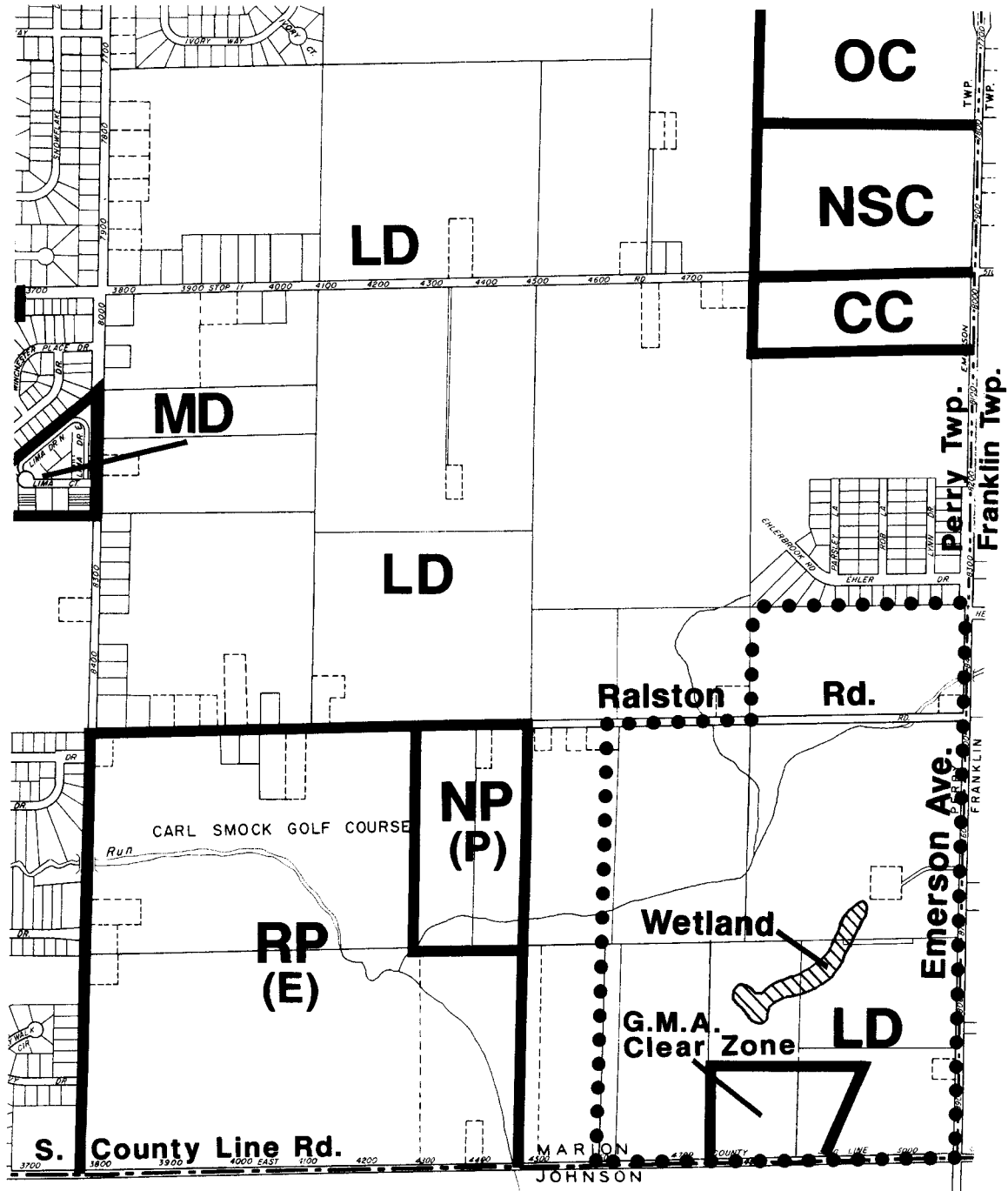
Emerson Avenue and South County Line Road are primary arterials. When development occurs, these roads will require widening.

The site contains a small wetland.
- Recommendations:
 - a. The Greenwood Municipal Airport has purchased the clear zone. This ownership can ensure conformance with airport plans and any applicable regulations. The airport can monitor nearby development activity and advise Indianapolis officials of any conflicts or concerns.

- b. Preserve the wetland as part of an open space area when the residential development occurs.
- c. When development occurs, dedicate right-of-way for road improvements.
- Additional Data:
 - a. Soil Limitations - The site's soils are Crosby-Brookston. These soils are characterized as somewhat poorly drained and very poorly drained. The area's topography is nearly level to gently sloping. Provide sewer services to new and existing development to prevent health hazards due to the soil associations' inability to accommodate septic systems.
 - b. Drainage - Due to the site's soils and topography, drainage needs may not be met when development occurs. Drainage should be thoroughly analyzed prior to development. Development may be accommodated through appropriate site grading, design, and off-site improvements as required by the City Drainage and Sediment Control Ordinance.
 - c. Environmental Constraints - A portion of the critical area is a wetland. When this area is considered for development, preserve the wetland.
 - d. Sanitary Sewers - The area is served by the South Marion County Interceptor. Contact the Department of Public Works' sewer permitting division when considering new or extended sewer service.
 - e. Water Facilities - Water service exists and is available to the area from water lines north and west of the critical area.
 - f. Transportation - The Thoroughfare Plan recommends South County Line Road be improved to a four-lane primary arterial connector.

(See Map 18, next page)

Map 18 - Critical Area 10



Conclusion

The Perry Township Comprehensive Land Use Plan was developed through a systematic and highly participatory process. City-County staff initiated the planning process, gathered and analyzed township data, and led planning committee meetings. The township planning committee worked with City-County staff to develop the final plan. The plan's recommendations reflect thorough examination of township population, land use characteristics, infrastructure, and environmental limitations as well as open discussion of planning and development goals. The recommendations of the plan map and plan narrative will help guide the future development of Perry Township.

The plan will be evaluated in the future to determine when there is a need for revision. Some of the factors that may indicate the need for a revision include significant changes in the operation and size of the Greenwood Municipal Airport, rapid development of all or part of the township, major changes in land use, significant increases in population, and extension of major sewer or water lines. Periodic revisions can be made via the adoption of new critical areas (see pages 18 and 19) or subarea plans, thereby keeping the Perry Township Plan current without development of an entirely new township plan.

Appendix A

Perry Township Planning Committee

The following is a list of Perry Township Planning Committee members who attended Committee Meetings. The planning committee was an open membership committee. The Division of Planning initiated committee formation by inviting all identifiable township leaders -- neighborhood organization presidents, school board members, City-County Council members, Metropolitan Development Commission members, and other community leaders. The Division relied upon community communication and also contacted local media to advertise the planning process and meetings.

Planning Committee meetings were held for the formation of this Perry Township Comprehensive Plan. The recommendations of this plan are the result of committee discussions and conclusions. Throughout the planning process the committee participated actively. The Division of Planning staff thanks each of the committee members for their invaluable participation and patience throughout the months of review and revision that resulted in this Perry Township Comprehensive Land Use Plan.

Anna Anderson	Robert Anderson
Robert Armstrong	L. Wendell Baker
Darrell Bakken	John S. Beeman
George Bixler	Kathleen Blackman
Patty Boger	Lawrence Borst
Dr. Philip Borst	Allan Brassard
Roy Buford	Tom Cady
David Caplinger	Bill Click
Jerry Cosby	Beulah Coughenour
Philip Crist	James Curtis, Jr.
Joe Daly	Raymond L. Fatheree
David C. Fenske	Ed Ferguson
Ellen Flexman	Doug Fortuna
Mary K. Gillum	Geri Gill
Carolyn Grant	Gene Hedges
Richard E. Hedges	Lois Horth
Pam Hume-Engle	Stephen Jones
George Junius	Richard Kriese
Gene Leeuw	C.L. Livengood
Marianne E. McCullough	Kevin McGinnis
David P. McGrath	Deloris J. Mennel
Mary Ann Mills	James Nuttall
Win F. Pulsifer	Kathy Price
Richard L. Rawlings	Statia Runyan
Jane Ryan	John Schattner

Appendix A (Continued)

Margaret Schattner
George Schmid
Wallace Sims
C. David Woerner

John Schimpf
Joe Sergi
Rosemarie Sylvester
Mark Yeager

Elected Officials

Stephen Goldsmith, *Mayor*

City-County Councilors and Districts

Gordon Gilmer, 1
William Schneider, 3
Linda Beadling, 5
Stuart Rhodes, 7
Glenn Howard, 9
Rozelle Boyd, 11
Cory O'Dell, 13
Mary B. Moriarty, 15
Jeff Golc, 17
Kenneth Giffin, 19
Frank T. Short, 21
David Smith, 23
Dr. Philip Borst, 25
Ron Franklin, AL
Stephen R. West, AL

Dr. Beurt SerVaas, 2
William Dowden, 4
Elwood E. Black, 6
Randy Shambaugh, 8
Paul Jones, 10
Betty Ruhmkorff, 12
Z. Mae Jimison, 14
Maggie Brents, 16
Phillip Hinkle, 18
Timothy M. Mullin, 20
Susan Williams, 22
Beulah Coughenour, 24
Carlton E. Curry, AL
W. Tobin McClamroch, AL

Administration and Policy Direction

Metropolitan Development Commission

William R. Brown
Jack Hall, M.D.
Mary Ann Mills
Julie P. Scott
James Wade, Jr.

James J. Curtis, Sr.
Lois J. Horth
Michael W. Rodman
Randolph L. Snyder

Project Coordination

Nancy Silvers, *Deputy Mayor and Acting Director for Dept. of Metropolitan Development*

Department of Metropolitan Development, Division of Planning

Leslie Rubin, Ph.D., *Administrator*

Thomas Bartlett, Senior Planner
Kira (Schmidt) Wauwie, AICP, Planner
Jay Getz, Planner

Phil Pettit, Drafting Superintendent
Darrel Walton, Draftsman
John Roberts, Draftsman
Kenneth Percy, Print Shop Manager
Burt Carter, Printer

Participating Agencies

City of Indianapolis Departments:
Metropolitan Development -
Division of Development Services
Division of Planning
Historic Preservation Commission
Parks and Recreation
Public Works
Transportation

Indiana Dept. of Natural Resources
Indiana Dept. of Transportation
Indiana Historic Landmarks Foundation
Indianapolis Water Company
Marion County Health Department